

DRILL HOLES

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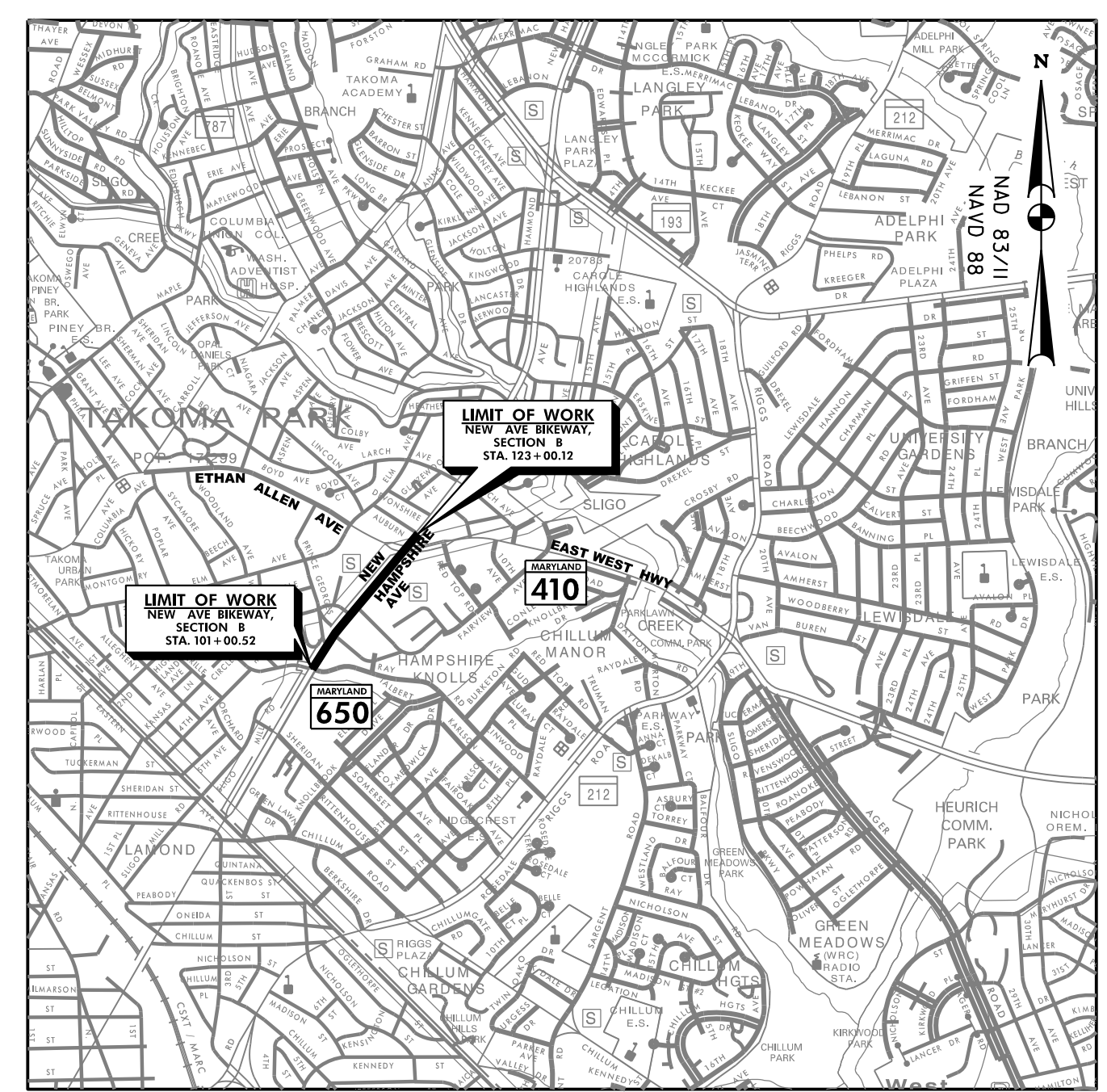
BY: KBOAMPONG

THE CITY OF TAKOMA PARK PLANNING AND COMMUNITY DEVELOPMENT

NEW AVE BIKEWAY, SECTION B MD 650 (NEW HAMPSHIRE AVENUE) POPLAR AVENUE TO AUBURN AVENUE SHA TRACKING NO. 20-AP-MO-20-xx

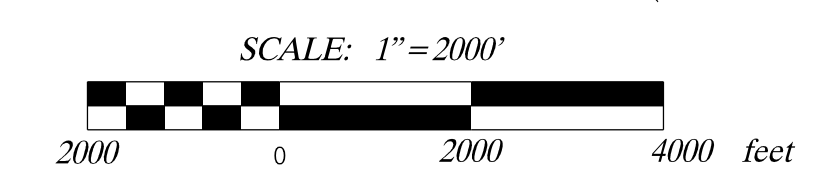
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MONTGOMERY COUNTY

LENGTH OF PROJECT:
NEW HAMPSHIRE AVENUE (MD 650) = 0.40 miles



HORIZONTAL DATUM	NAD 8311
VERTICAL DATUM	NAVD 88
WSSC TWO-HUNDRED FOOT SHEET NUMBER:	208NE01

60% SUBMISSION
NOVEMBER 2021

AASHTO DESIGN CRITERIA

THIS PROJECT WAS DESIGNED IN ACCORDANCE WITH THE 2018 (7TH EDITION) PUBLICATION OF AASHTO'S "A POLICY ON GEOMETRIC DESIGN OF HIGHWAYS AND STREETS."

STANDARD SPECIFICATIONS BOOK, BOOK OF STANDARDS AND MUTCD

ALL WORK ON THIS PROJECT SHALL CONFORM TO THE LATEST APPROVED MARYLAND DEPARTMENT OF TRANSPORTATION STATE HIGHWAY ADMINISTRATION (MDOT SHA) "STANDARD SPECIFICATIONS FOR CONSTRUCTION AND MATERIALS" REVISIONS THEREOF OR ADDITIONS THERETO AS INDICATED IN THE PROJECT DESCRIPTION OF THE INVITATIONS FOR BIDS BOOK; THE SPECIAL PROVISIONS INCLUDED IN THE INVITATION FOR BIDS BOOK; THE ADMINISTRATION'S "BOOK OF STANDARDS FOR HIGHWAYS AND INCIDENTAL STRUCTURES" AND THE LATEST ADOPTED MUTCD.

RIGHT OF WAY

RIGHT OF WAY AND EASEMENT LINES SHOWN ON THESE PLANS ARE FOR ASSISTANCE IN INTERPRETING THE PLANS. THEY ARE NOT OFFICIAL. FOR OFFICIAL FEE RIGHT OF WAY AND EASEMENT INFORMATION, SEE THE APPROPRIATE RIGHT OF WAY PLATS.

UTILITIES

THE LOCATION OF UTILITIES SHOWN ON THE PLANS ARE FOR INFORMATION AND GUIDANCE ONLY. NO GUARANTEE IS MADE OF THE ACCURACY OF SAID LOCATIONS.

COMPLETENESS OF DOCUMENTS

THE CITY OF TAKOMA PARK SHALL ONLY BE RESPONSIBLE FOR THE COMPLETENESS OF DOCUMENTS OBTAINED DIRECTLY FROM THE STATE HIGHWAY ADMINISTRATION'S CASHIER'S OFFICE. FAILURE TO ATTACH ADDENDA MAY CAUSE THE BID TO BE IRREGULAR.

ADA COMPLIANCE

THE DESIGN OF THIS PROJECT HAS INCORPORATED FACILITIES FOR THE ELDERLY AND HANDICAPPED IN COMPLIANCE WITH THE STATE AND FEDERAL LEGISLATION.

ENVIRONMENTAL INFORMATION

ALL STORMWATER MANAGEMENT FACILITIES CONSTRUCTED FOR THIS CONTRACT SHALL BE INSPECTED AND MAINTAINED IN ACCORDANCE WITH THE CITY OF TAKOMA PARK MUNICIPAL CODE TITLE 16 (SECTIONS 16.04.210 THROUGH 16.04.260).

SEDIMENT AND EROSION CONTROL REGULATIONS WILL BE STRICTLY ENFORCED DURING CONSTRUCTION.

FOLLOWING INITIAL SOIL DISTURBANCE OR REDISTURBANCE PERMANENT OR TEMPORARY STABILIZATION SHALL BE COMPLETED WITHIN THREE (3) CALENDER DAYS AS TO THE SURFACE OF ALL PERIMETER CONTROLS, DIKES, SWALES, DITCHES, PERIMETER SLOPES, AND ALL SLOPES GREATER THAN 3 HORIZONTAL TO 1 VERTICAL (3:1), AND SEVEN DAYS (7) AS TO ALL OTHER DISTURBED OR GRADED AREAS ON THE PROJECT SITE.

OWNERS / DEVELOPERS CERTIFICATION :

I / WE HEREBY CERTIFY THAT ANY CLEARING, GRADING, CONSTRUCTION AND/OR DEVELOPMENT WILL BE DONE PURSUANT TO THIS PLAN, AND THAT ANY RESPONSIBLE PERSONNEL INVOLVED IN THE CONSTRUCTION PROJECT WILL HAVE A CERTIFICATE OF ATTENDANCE AT A MARYLAND DEPARTMENT OF THE ENVIRONMENT APPROVED TRAINING PROGRAM FOR THE CONTROL OF SEDIMENT AND EROSION BEFORE BEGINNING THE PROJECT. I HEREBY AUTHORIZE THE RIGHT OF ENTRY FOR PERIODIC ON-SITE EVALUATION BY STATE OF MARYLAND, DEPARTMENT OF THE ENVIRONMENT, COMPLIANCE INSPECTORS.

Signature _____ DATE _____
Rosalind Grigsby Community Development Manager
(301) 891-7205 City of Takoma Park
Printed Name and Title

REVISIONS	

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ABBREVIATIONS

AASHTO	American Association of State Highway Transportation Officials	HDWL.....	Headwall	RW or RW...	Right of Way
ADT.....	Average Daily Traffic	HERCP.....	Horizontal Elliptical Reinforced Concrete Pipe	RCP	Reinforced Concrete Pipe
AHD.....	Ahead	HP.....	High Point	RCPP	Reinforced Concrete Pressure Pipe
APPROX.....	Approximate	IN.....	Inch	R.Q.D.	Rock Quality Designation
B or BL.....	Baseline	I.S.T.....	Inlet Sediment Trap	R.M.	Rootmat
BK	Back /Book	INV.....	Invert	S	South
BIT.	Bituminous	J.B.....	Junction Box	SAN.	Sanitary Sewer
B.C.....	Bituminous Concrete	K	K Inlet	SB or SB	Southbound
B.M.....	Bench Mark	L	Length	S.D.	Storm Drain
BOT.....	Bottom	LF	Linear Feet	S.D.D.	Surface Drain Ditch
C.C.....	Center of Curve	L.L.....	Liquid Limit	SE	Super Elevation
CAP.....	Corrugated Aluminum Pipe	LP	Low Point	SF	Silt Fence
CAPA	Corrugated Aluminum Pipe Arch	L.P.....	Light Pole	SF	Square Feet
CATV	Cable Television	LT.....	Left	SHT.	Sheet
C.B.R.....	California Bearing Ratio	MAC.....	Macadam	SPP	Structural Steel Plate Pipe
CL or CL.....	Centerline	M.C.....	Moisture Content	SPPA	Structural Steel Plate Pipe Arch
CL	Class	MAX.....	Maximum	S.P.T.....	Standard Penetration Testing
CLF.....	Chainlink Fence	M.D.D.....	Maximum Dry Content	SRP	Steel Spiral Rib Pipe – Aluminized Type 2
CMP.....	Corrugated Metal Pipe	MOD.....	Modified	SRPA	Steel Spiral Rib Pipe Arch – Aluminized Type 2
C.O.....	Cleanout	MIN.....	Minimum	SSD	Stopping Sight Distance
COMB.....	Combination	N.....	North	SSF	Super Silt Fence
CONC.....	Concrete	NB	Northbound	STD.	Standard
CONSTR.	Construction	NE	Northeast	STA.	Station
COR.....	Corner	N.P.....	Non-Plastic	SO.	Single Opening
CORR.....	Correction	O.C.....	On Center	SY	Square Yards
CPP-S	Corrugated Polyethylene Pipe – Type ‘S’	OHE	Overhead Electric	SWM.....	Stormwater Management
CSP	Corrugated Steel Pipe – Aluminized Type 2	O.M.....	Optimum Moisture	T	Tangent
CSPA	Corrugated Steel Pipe Arch – Aluminized Type 2	PAV T.....	Pavement	T	Telephone
DC.....	Degree of Curve	PC	Point of Curvature	T.C.....	Top of Cover
D.H.V.....	Design Hourly Volume	PCC	Point of Compound Curvature	T.G.....	Top of Grate
D.I.....	Drop Inlet	PC	Point of Crown	T or TL	Traverse Line
DIA.....	Diameter	PGE	Profile Grade Elevation	T.M.....	Top of Manhole
D.O.....	Double Opening	P.G.E.....	Profile Ground Elevation	TRAV.....	Traverse
E	East	P.G.L.....	Profile Grade Line	TS	Temporary Swale
E	Electric	PGL	Profile Ground Line	T.S.....	Top of Slab
E	External Distance	P/R	Point of Rotation	T.S.....	Topsoil
EA	Each	P.I.....	Plasticity Index	TYP.....	Typical
EB	Eastbound	PI	Point of Intersection	U.D.....	Under Drain
ELEV	Elevation	POC	Point On Curve	U.G.....	Underground
ES	End Section	POT	Point On Tangent	U.P.....	Utility Pole
EX or EXIST ..	Existing	PPWP	Polyvinyl Chloride Profile Wall Pipe	USDA	United States Department of Agriculture
FT	Feet	PROP	Proposed	VCL	Vertical Clearance
F or FL.....	Flowline	PRC	Point of Reverse Curve	V.C.L.....	Vertical Curve Length
F.B.D.	Flat Bottom Ditch	PT	Point	W	Water
F.H.....	Fire Hydrant	PT	Point of Tangency	W	West
FWD.....	Forward	PVC	Point of Vertical Curve	WB	Westbound
G	Gas	PVC	Polyvinyl Chloride	WB	Wetland Buffer
G.V.....	Gas Valve	PVI	Point of Vertical Intersection	W.M.....	Water Meter
H.B.....	Handbox	PVRC	Point of Vertical Reverse Curve	W.S.....	Wrapped Steel
HDPE	High Density Polyethylene	PVT	Point of Vertical Tangency	WUS	Waters of the United States
		R	Radius	W.V.....	Water Valve
		R.F.....	Rock Fragments		
		RT	Right		

GENERAL NOTES

1. THE EXISTING UTILITIES AND OBSTRUCTIONS SHOWN ON THESE PLANS ARE FROM THE BEST AVAILABLE RECORDS AND SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION.THE CONTRACTOR SHALL NOTIFY ALL UTILITY OWNERS CONCERNED AND MISS UTILITY PRIOR TO CONSTRUCTION ACTIVITIES.THE CONTRACTOR SHALL CALL "MISS UTILITY" AT LEAST 48 HOURS IN ADVANCE OF ANY EXCAVATION WORK AT 1-800-257-7777.
2. THE CONTRACTOR SHALL PROTECT AND NOT INTERRUPT EXISTING UTILITY SERVICES UNLESS OTHERWISE NOTED ON THE PLANS OR AUTHORIZED BY THE ENGINEER. SEE UTILITY STATEMENT.
3. THE CONTRACTOR IS RESPONSIBLE TO ENSURE THAT THE SAFETY OF THE PUBLIC AND ALL WORKERS IS MAINTAINED AT ALL TIMES THROUGHOUT THE TERM OF THE CONTRACT. MOTORISTS SHALL BE GUIDED IN A CLEAR AND POSITIVE MANNER WHILE APPROACHING AND PASSING THROUGH CONSTRUCTION WORK AND EQUIPMENT AREAS.
4. HORIZONTAL CONTROL: THE LOCATION AND ELEVATION OF BENCH MARKS ARE SHOWN ON THE PLANS. ALL ELEVATIONS ARE IN FEET AND ARE BASED ON THE NAVD 88.
5. WHERE REFERENCE IS MADE TO MDOT SHA STANDARD PLATES IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO HAVE IN HIS POSSESSION THE LATEST UP-TO-DATE STANDARD PLATES AS OF THE DATE OF ADVERTISEMENT OF THESE PLANS. STANDARD PLATES ARE AVAILABLE AT WWW.MARYLANDROADS.COM.
6. THE CONTRACTOR SHALL GRADE FOR POSITIVE DRAINAGE AT ALL ROADWAY INTERSECTIONS, ENTRANCES AND YARDS.
7. REPAIRS TO UTILITIES OR PROPERTY DAMAGE AS A RESULT OF CONTRACTOR'S NEGLIGENCE OR METHOD OF OPERATION SHALL BE MADE AT NO ADDITIONAL COST TO THE CITY, THE ADMINISTRATION OR THE OWNER.
8. PROVIDE 4-INCH FURNISHED TOPSOIL AND TURFGRASS SOD ESTABLISHMENT ON SLOPES UNLESS OTHERWISE NOTED ON THE PLANS.
9. MATERIAL REMOVED DURING CONSTRUCTION SHALL BECOME THE CONTRACTOR'S PROPERTY UNLESS OTHERWISE NOTED ON THE PLANS OR IN THE SPECIAL PROVISIONS.
10. THE CONTRACTOR SHALL RESET ANY SIGN POSTS OR MAIL BOXES TO FACILITATE THE WORK,EXCEPT WHERE SPECIFIED ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
11. FINAL DETERMINATION AS TO THE LOCATION OF EROSION AND SEDIMENT CONTROLS WILL BE AT THE DIRECTION OF THE ENGINEER WHO RESERVES THE RIGHT TO ORDER ADDITIONAL E&S DEVICES.
12. CONSTRUCTION EQUIPMENT SHALL HAVE TREADS/TIRES CLEANED PRIOR TO LEAVING THE LOD.ALL MATERIAL REMOVAL/LOAD OUT SHALL BE LIFTED FROM THE LOD.ALL SEDIMENT SPILLED,DROPPED OR TRACKED ONTO THE ROAD MUST BE REMOVED IMMEDIATELY BY VACUUMING,SCRAPING OR SWEEPING.
13. SEVERAL PROPOSED DRAINAGE STRUCTURES AND PIPES WILL CONNECT TO EXISTING STORM DRAIN STRUCTURES AND PIPES.THE CONTRACTOR SHALL FIELD VERIFY INVERTS PRIOR TO ORDERING,FABRICATING OR CONSTRUCTING PROPOSED STORM DRAIN STRUCTURES.
14. SAW CUTS WILL NOT BE MEASURED BUT WILL BE INCIDENTAL TO OTHER RELATED ITEMS AS SPECIFIED IN THE CONTRACT DOCUMENTS.
15. VERTICAL ADJUSTMENT OF EXISTING UTILITIES SHALL BE INCIDENTAL TO THE 5 INCH CONCRETE SIDEWALK,SPECIALTY PAVERS – TYPE 2 OR ASPHALT SHARED USE PATH PAY ITEMS. SEE SP 603 – SIDEWALKS.

CONVENTIONAL SIGNS (SAMPLES)

PROPOSED MEDIAN BARRIER		PROPOSED PIPE / CULVERT	
ELECTRICAL HAND BOX – SIGNALS		EXISTING PIPE / CULVERT	
FLOW LINE		EXISTING DROP INLET	
STATE,COUNTY OR CITY LINES		UTILITY POLE	
PROPOSED TRAFFIC BARRIER		WETLAND	
EXISTING TRAFFIC BARRIER		WETLAND BUFFER	
PROPOSED FENCE LINE		WATERS OF THE U.S.	
EXISTING FENCE LINE		HEDGE /TREE LINE	
RIGHT OF WAY LINE		BUSH /TREE	
EXISTING ROADWAY		CONIFEROUS TREE	
RAILROAD		GROUND ELEVATION	
BASE LINE OR SURVEY LINE		GRADE ELEVATION	
FIRE HYDRANT		ROADWAY BORING PLAN LOCATION	
HISTORIC BOUNDARY		UTILITY TEST HOLE LOCATION	
WATERS OF THE U.S.			
WETLAND BOUNDARY			

AB-01

CITY OF TAKOMA PARK
NEW AVE BIKEWAY, SECTION B
MD 650 (NEW HAMPSHIRE AVENUE)
POPLAR AVE TO AUBURN AVE
SHA TRACKING NO. 20-AP-MO-020-XX

REVISIONS	ABBREVIATIONS AND GENERAL NOTES	
60% PLANS NOVEMBER 2021	SCALE <u> N.T.S. </u> DATE <u> NOVEMBER 2021 </u> CONTRACT NO. <u> T.B.D. </u>	
	DESIGNED BY <u> SAB </u>	COUNTY <u> MONTGOMERY </u>
	DRAWN BY <u> KBJ </u>	
	CHECKED BY <u> RJG </u>	
	F.A.P. NO. <u> T.B.D. </u>	
	DRAWING NO. AB-01	1 OF 1 SHEET NO. 2 OF 44

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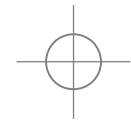
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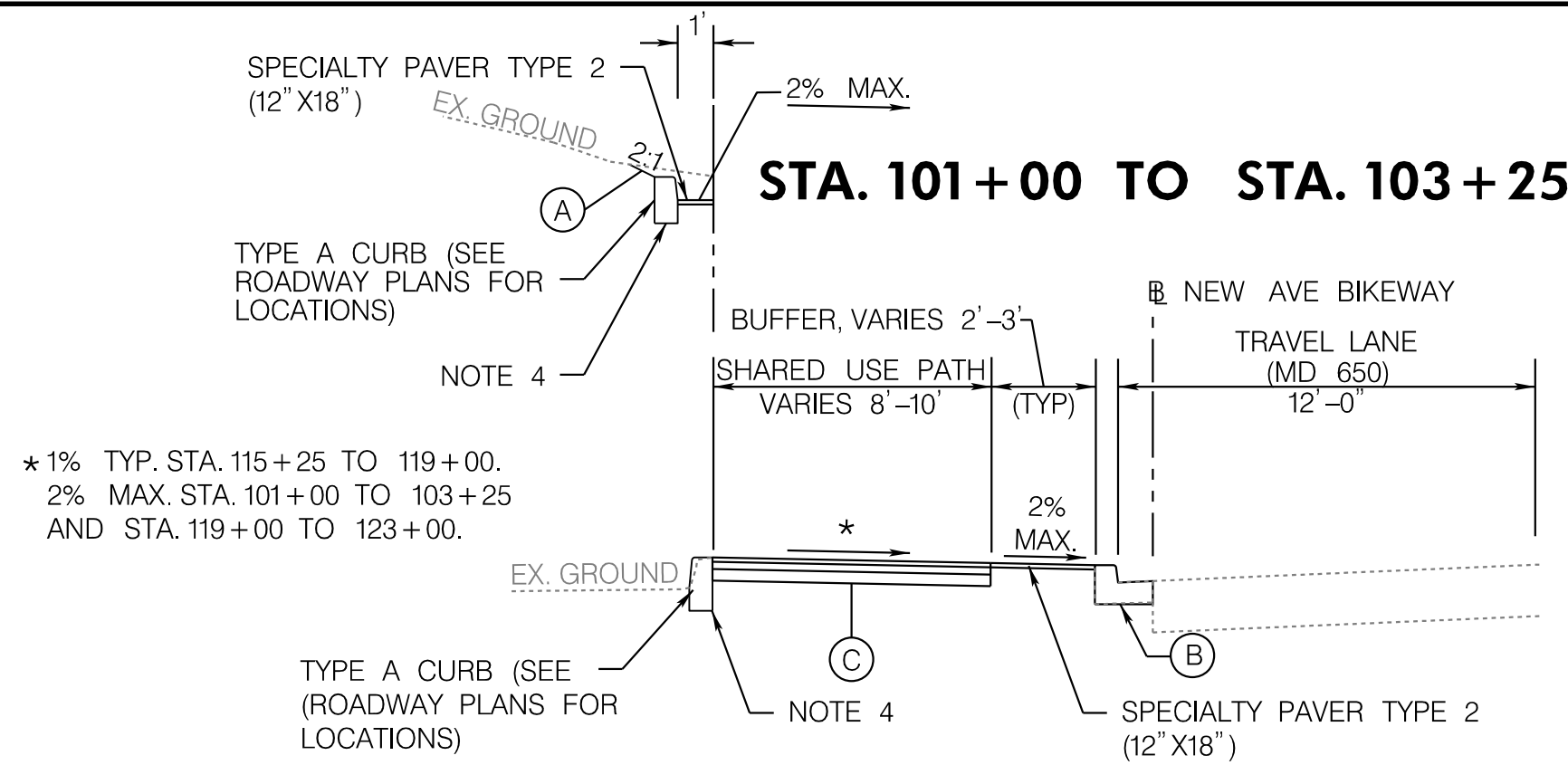
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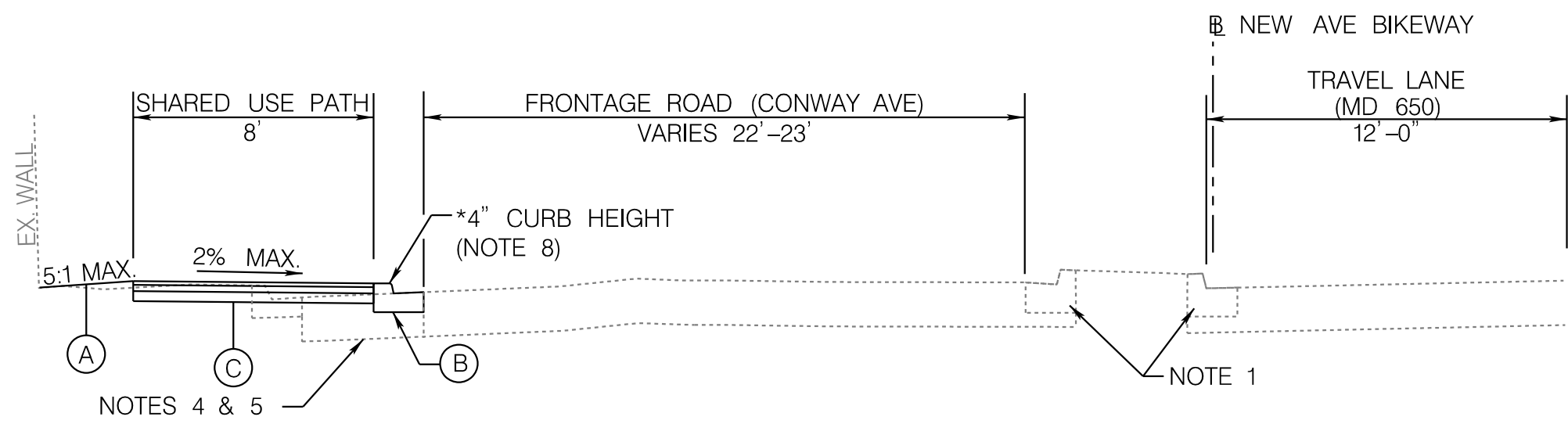
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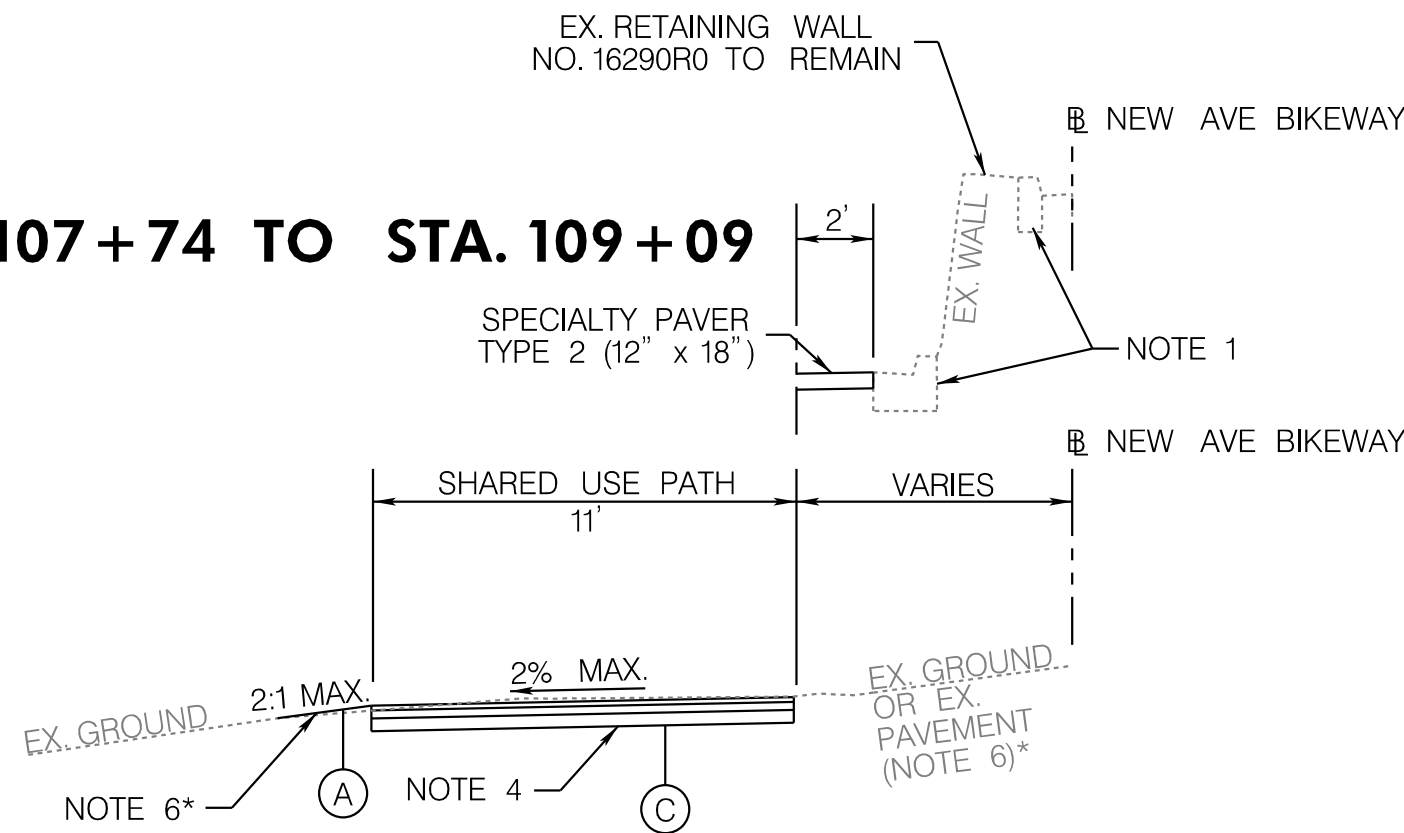


NEW AVENUE BIKEWAY
STA. 101+00 TO STA. 103+25
STA. 115+25 TO STA. 123+00



NEW AVENUE BIKEWAY
STA. 103+25 TO STA. 105+25

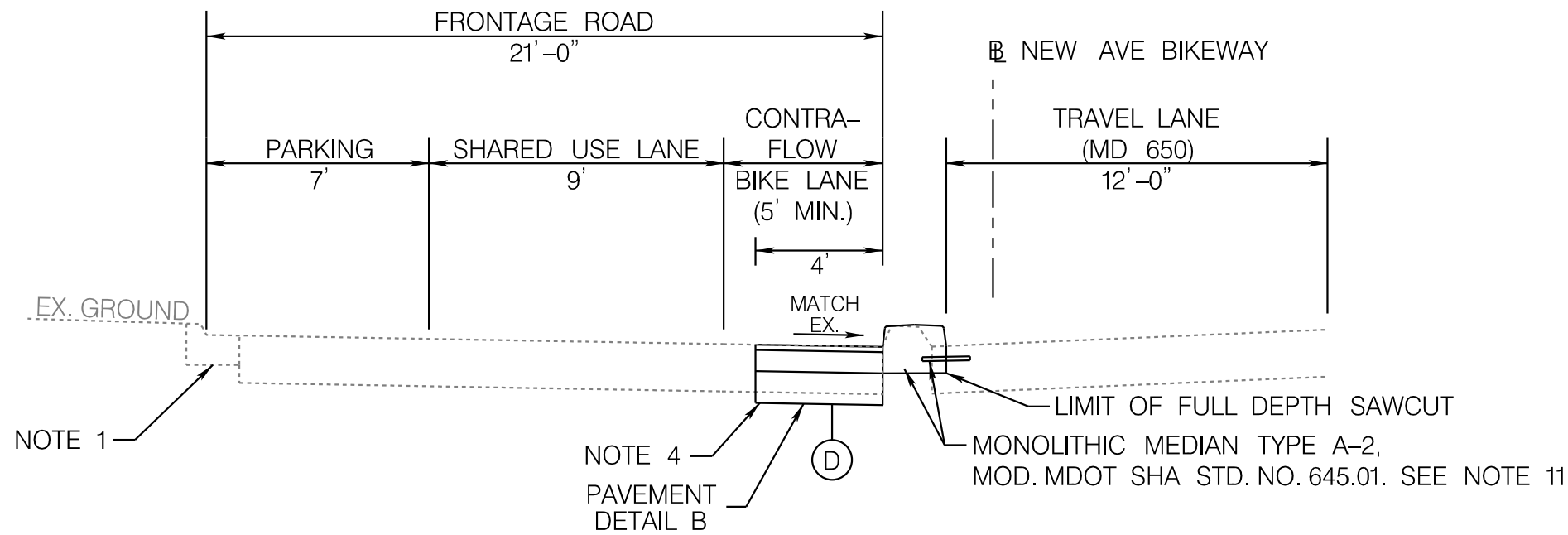
STA. 107+74 TO STA. 109+09



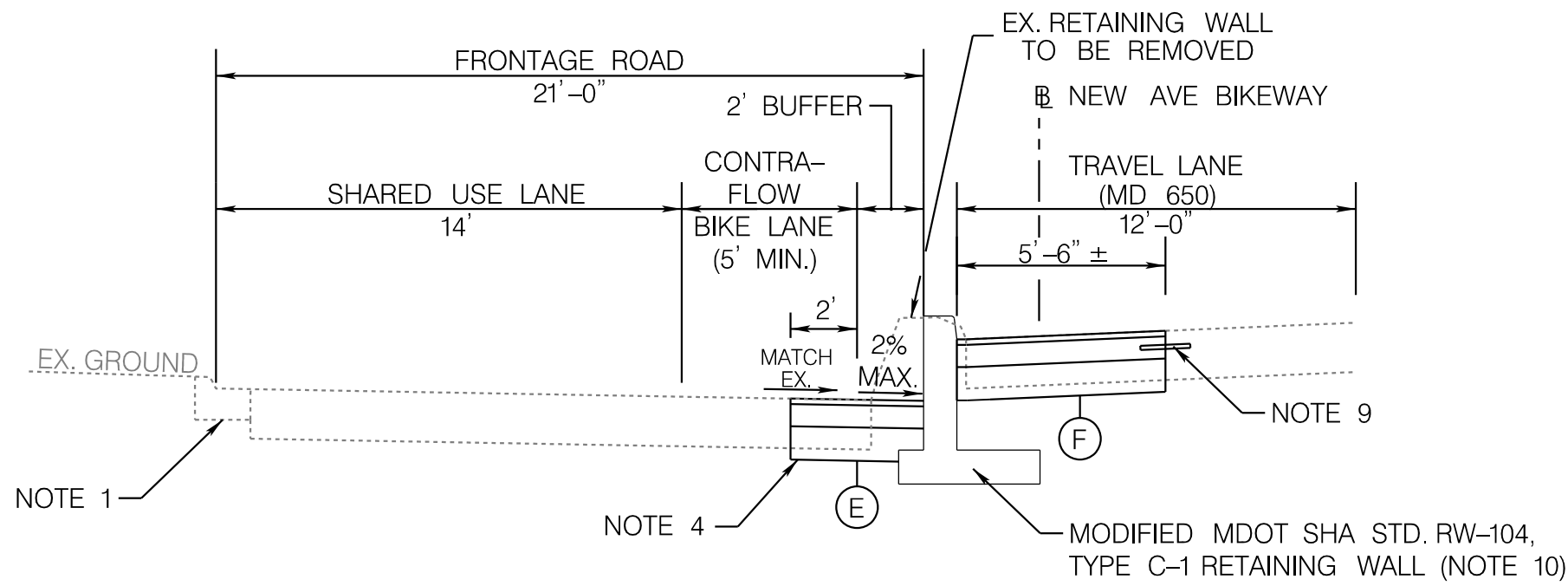
NEW AVENUE BIKEWAY
STA. 105+25 TO STA. 109+50

TYPICAL SECTION LEGEND

- (A) 4" FURNISHED TOPSOIL & TURFGRASS SOD ESTABLISHMENT
- (B) MDOT SHA STANDARD TYPE A COMBINATION CURB AND GUTTER, ANY HEIGHT OR DEPTH, PAVEMENT DETAIL NOTE 6, SHEET DT-01
- (C) MDOT SHA STD. NO. 580.08: BIKE PATHS - FLEXIBLE PAVEMENT SECTION
- (D) SEE PAVEMENT DETAIL B, SHEET DT-01
- (E) SEE PAVEMENT DETAIL C, SHEET DT-01
- (F) SEE PAVEMENT DETAIL E, SHEET DT-01



NEW AVENUE BIKEWAY
STA. 109+50 TO STA. 113+25



NEW AVENUE BIKEWAY
STA. 113+25 TO STA. 115+25

NOTES:

- EXISTING CURB AND GUTTER TO REMAIN.
- SAW CUTS ARE INCIDENTAL TO THE EXCAVATION OR PAVING ITEMS.
- ALL ROADWAY EXCAVATION SHALL BE DEFINED AS CLASS 1 REGARDLESS OF THE WIDTH OF THE EXCAVATION.
- LIMIT OF CLASS 1 EXCAVATION AND TOP OF SUBGRADE.
- EXCAVATE TO THE TOP OF EXISTING SUBGRADE. BACKFILL WITH COMMON BORROW TO BOTTOM OF SIDEWALK OR ASPHALT SHARED USE PATH.
- EXCAVATE TO THE TOP OF EXISTING SUBGRADE. BACKFILL WITH FURNISHED SUBSOIL TO 4" BELOW PROPOSED GRADE. PLACE 4" FURNISHED TOPSOIL TO PROPOSED GRADE. PROVIDE TURFGRASS SOD ESTABLISHMENT.
- SEE SIGNING AND PAVEMENT MARKING PLANS FOR BIKE PAVEMENT STRIPING.
- ALL CONCRETE CURB AND GUTTER WILL BE PAID FOR AS STANDARD TYPE A COMBINATION CURB & GUTTER, ANY HEIGHT OR DEPTH.
- PROVIDE STEEL DOWELS 1.5" X 18", 1' ON CENTER, SIMILAR TO MDOT SHA TYPE 1 REPAIRS. PLACE DOWELS ALONG THE PERIMETER OF THE NEW CONSTRUCTION AND ALONG ANY EXISTING TRANSVERSE PAVEMENT JOINT.
- RETAINING WALL SHALL BE MODIFIED FROM STD. RW-104, TYPE C-1 AS FOLLOWS:

WALL REVEAL ADJACENT TO SOUTHBOUND MD 650 TRAVEL LANES SHALL NOT EXCEED 9 INCHES.

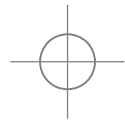
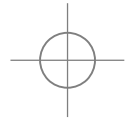
- NOTE 10
CONT'D.
- WALL REVEAL ADJACENT TO FRONTAGE ROAD SHALL NOT EXCEED 3 FEET.
- NO SUBGRADE DRAINAGE SYSTEM SHALL BE PROVIDED AS THE FINISHED GROUND LINE ON BOTH SIDES OF THE RETAINING WALL SHALL BE PAVED.
11. LONGITUDINAL TIE DEVICES SHALL BE PROVIDED ON THE SOUTHBOUND MD 650 SIDE ONLY, FOLLOWING MDOT SHA STD. NO. 572.61-01. TIE DEVICES SHALL BE STEEL DOWELS 1.5" X 18", 1' ON CENTER. PLACE DOWELS ALONG THE PERIMETER OF THE NEW CONSTRUCTION AND ALONG ANY EXISTING TRANSVERSE PAVEMENT JOINT.

TS-01

CITY OF TAKOMA PARK
NEW AVE BIKEWAY, SECTION B
MD 650 (NEW HAMPSHIRE AVENUE)
POPLAR AVE TO AUBURN AVE
SHA TRACKING NO. 20-AP-MO-020-XX

REVISIONS	TYPICAL SECTIONS		
60% PLANS NOVEMBER 2021	SCALE 1"=20' DATE NOVEMBER 2021 CONTRACT NO. T.B.D.		
	DESIGNED BY SAB	COUNTY MONTGOMERY	
	DRAWN BY SAB	LOGMILE MD 650 0.040-0.830	
	CHECKED BY RJG	F.A.P. NO. T.B.D.	
DRAWING NO. TS-01		1 OF 1	SHEET NO. 3 OF 44

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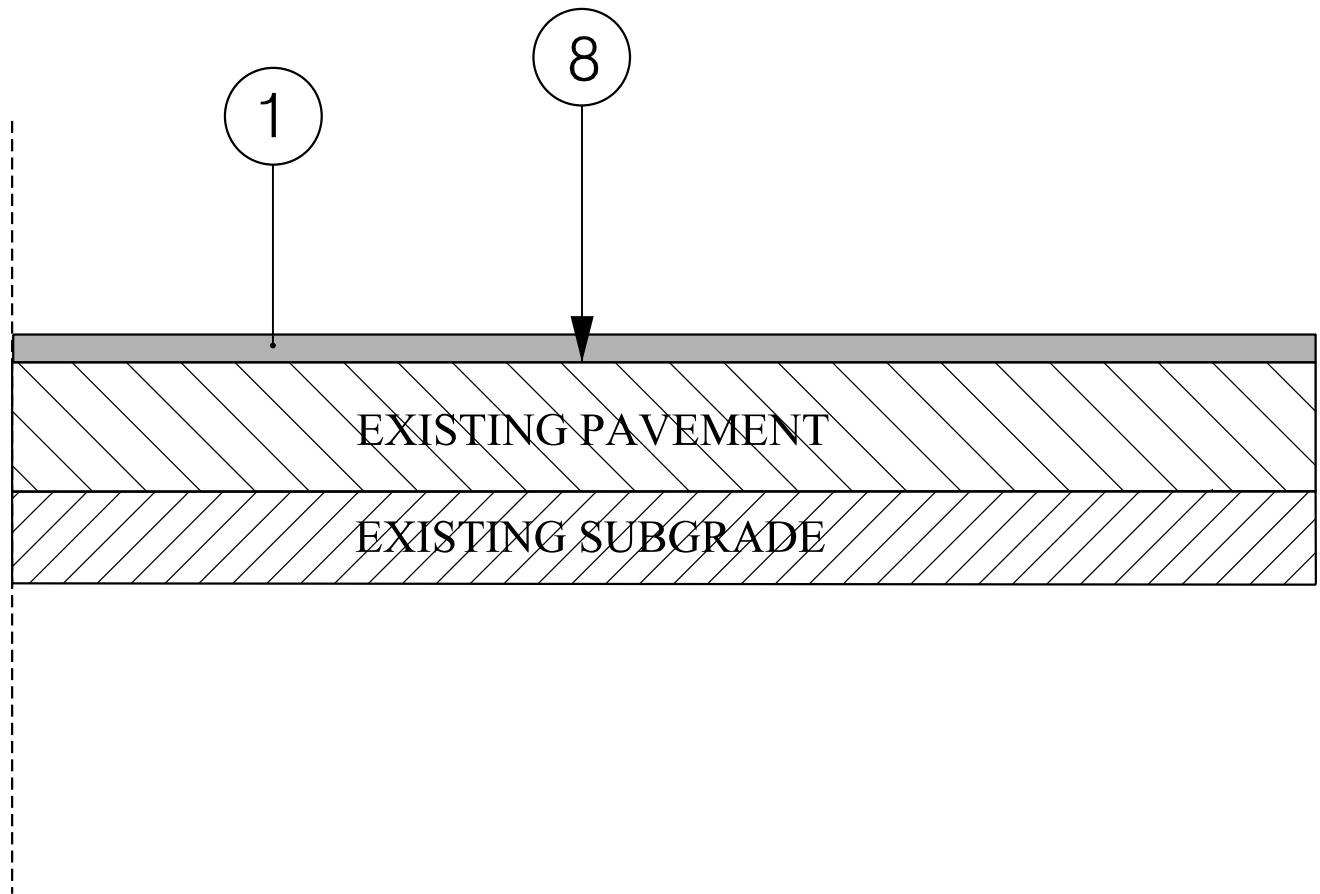
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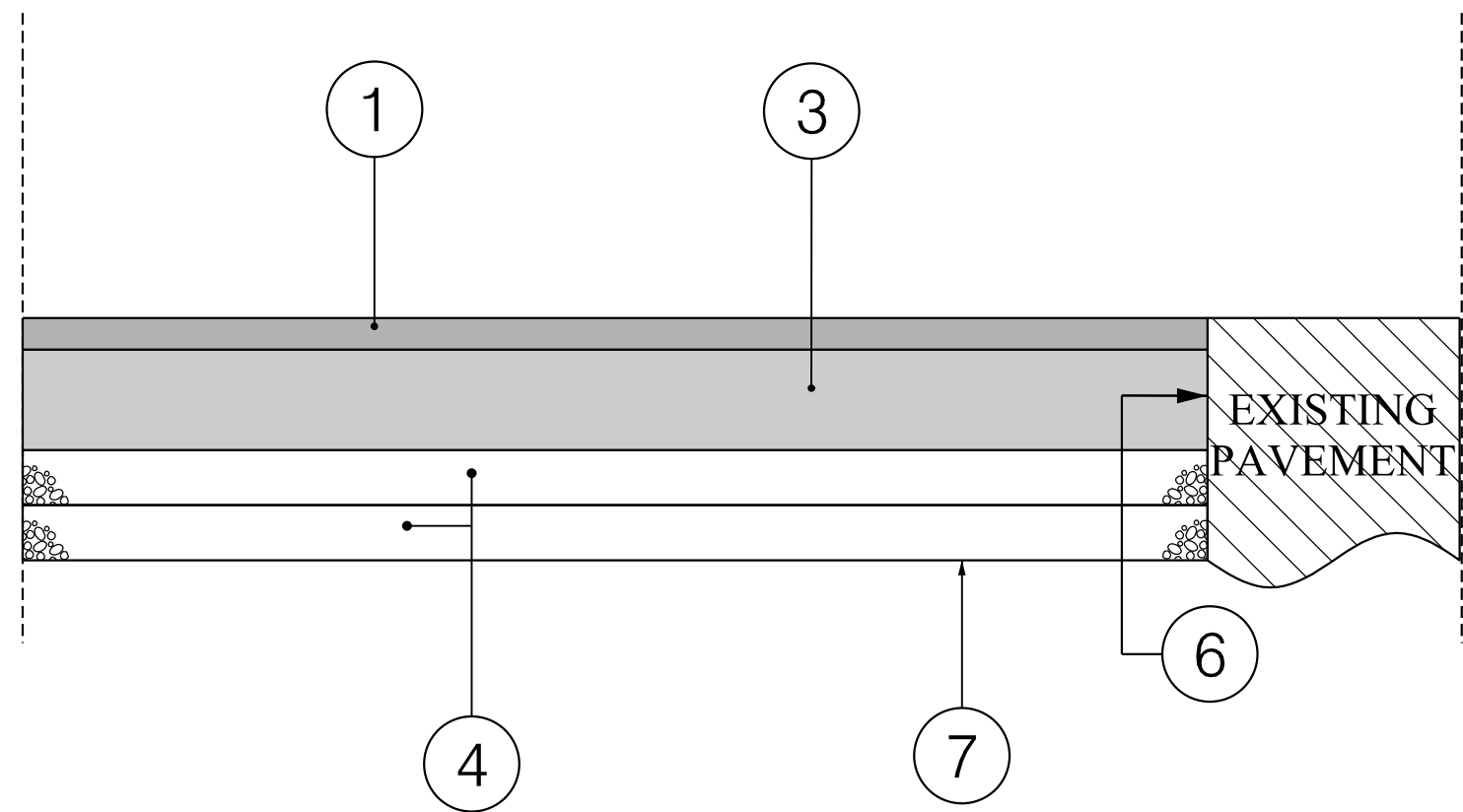
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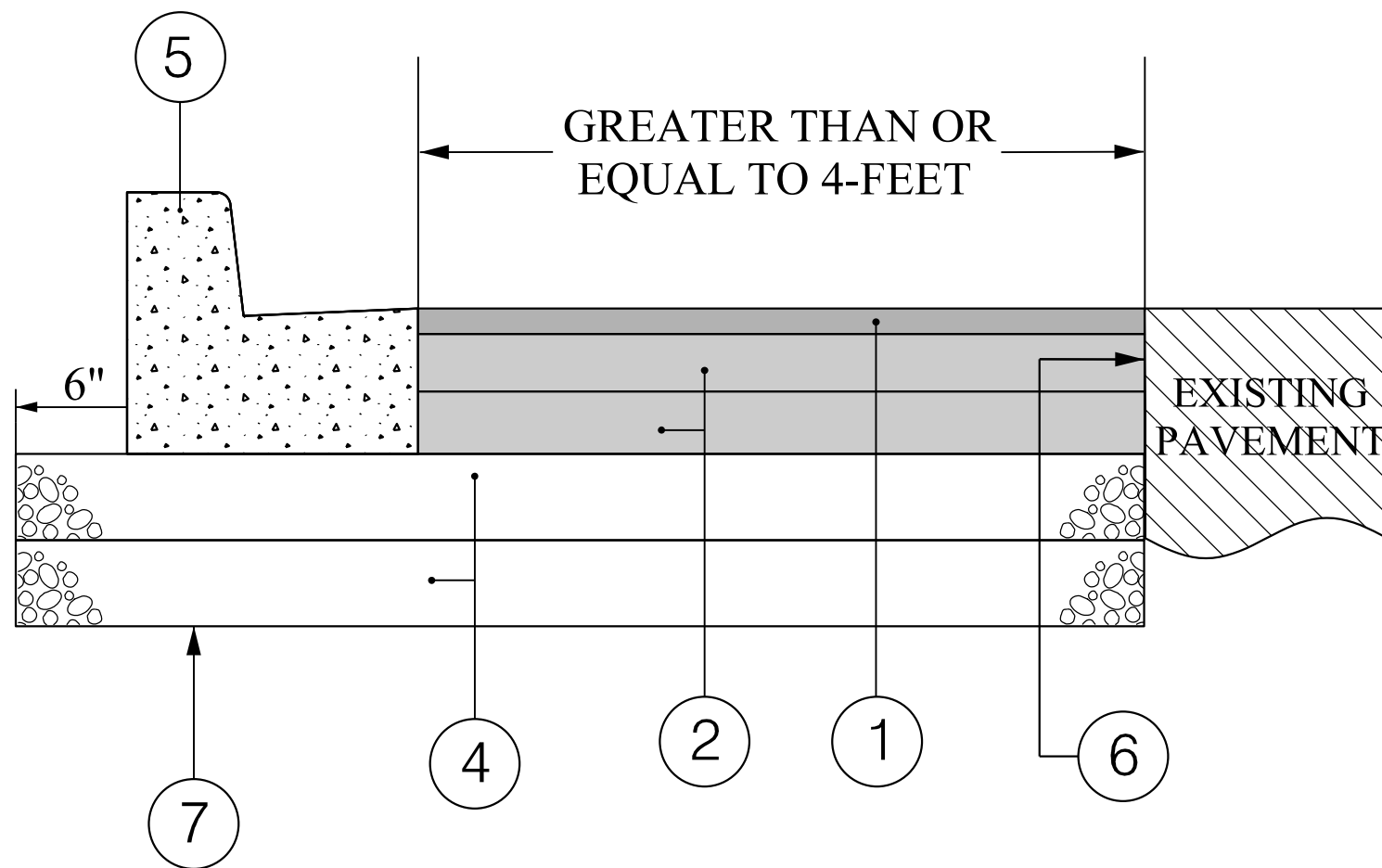
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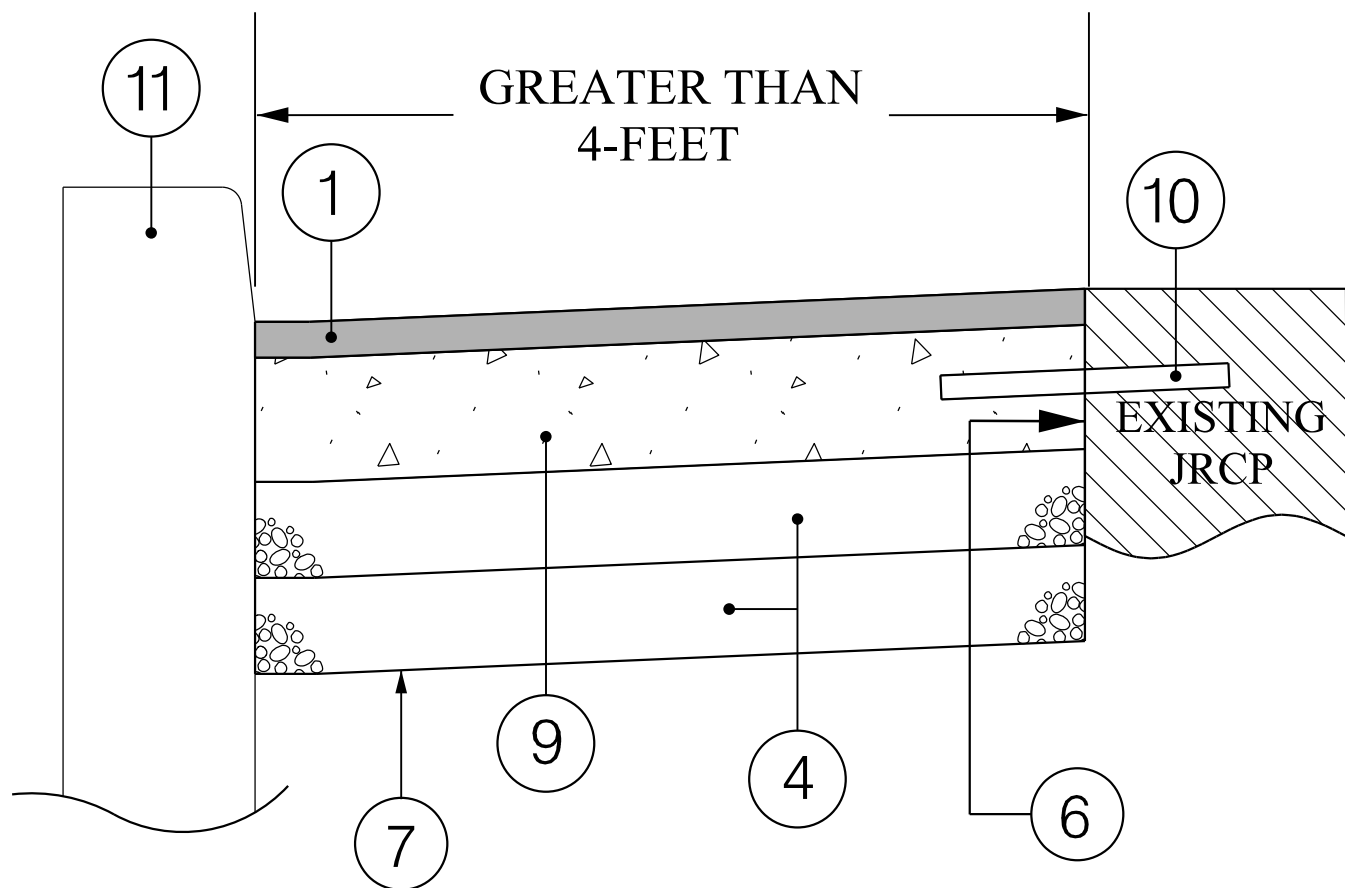
FINE MILLING AND RESURFACING
PAVEMENT DETAIL A



ASPHALT SECTION FOR FRONTAGE ROAD
PAVEMENT DETAIL B



BASE WIDENING, FLEXIBLE PAVEMENT
HEAVY DUTY PAVEMENT SECTION
PAVEMENT DETAIL C



BASE WIDENING, CONCRETE PAVEMENT
HEAVY DUTY PAVEMENT SECTION
PAVEMENT DETAIL E

PAVEMENT LEGEND

- ① 2" SUPERPAVE ASPHALT MIX 12.5 mm FOR SURFACE, HDFV, PG64S-22, LEVEL 2
- ② 4" SUPERPAVE ASPHALT MIX 19.0 mm FOR BASE, PG 64S-22, LEVEL 2
- ③ 3" SUPERPAVE ASPHALT MIX 19.0 mm FOR BASE, PG 64S-22, LEVEL 2
- ④ 6" GRADED AGGREGATE BASE COURSE
- ⑤ MDOT SHA STANDARD TYPE A COMBINATION CURB AND GUTTER, MONOLITHIC MEDIAN TYPE A-2, OR CAST-IN-PLACE RETAINING WALL (SEE PLANS)
- ⑥ FULL-DEPTH SAW CUT INCIDENTAL TO FULL-DEPTH PAVING, CURB AND GUTTER AND EXCAVATION ITEMS
- ⑦ TOP OF SUBGRADE AND LIMIT OF EXCAVATION (SEE NOTE 2)
- ⑧ TOP OF EXISTING PAVEMENT AFTER 2" FINE MILLING
- ⑨ 9" PLAIN PORTLAND CEMENT CONCRETE MIX NO. 7
- ⑩ LONGITUDINAL TIE DEVICE. SEE NOTE 9
- ⑪ CAST-IN-PLACE RETAINING WALL, MONOLITHIC MEDIAN, OR MDOT SHA STANDARD TYPE B CURB (SEE PLANS)

PAVEMENT DETAIL NOTES

- REMOVE AND DISPOSE OF ALL SOFT AND UNSTABLE MATERIAL PER SECTION 208 OF THE MDOT SHA STANDARD SPECIFICATIONS FOR CONSTRUCTION AND MATERIALS.
- 1A. BASED ON AS-BUILT PLANS, MD 650 HAS THE FOLLOWING APPROXIMATE PAVEMENT SECTION:

3" ASPHALT OVER 9" JOINTED REINFORCED CONCRETE PAVEMENT OVER 6" SUB-BASE.

BASED ON PAVEMENT CORES TAKEN ON JANUARY 22, 2021, THE FRONTAGE ROADS HAVE THE FOLLOWING PAVEMENT SECTIONS:

CORE C-1, PRINCE GEORGES AVE.: 4.5" ASPHALT OVER 12" GRAVEL BASE

CORE C-2, NEW HAMPSHIRE AVE. SHOPPING CENTER: 4" ASPHALT OVER 5.6" GRAVEL BASE
- IN AREAS WHERE EXISTING PAVEMENT IS BEING REMOVED, THE LIMIT OF EXCAVATION SHALL BE AT THE BOTTOM OF THE BOUND MATERIALS IN THE EXISTING PAVEMENT OR AT THE TOP OF SUBGRADE, WHICHEVER IS LOWER.
- FOR ASPHALT SHARED USE PATH, REFER TO MDOT SHA STD. NO. 580.08 FOR BIKE PATHS - FLEXIBLE PAVEMENT SECTION.
- REFER TO MDOT SHA STD. NO. 578.01 FOR REPAIRING PAVEMENT OPENINGS WITHIN UTILITY/STORM DRAIN TRENCHES. PAVEMENT REPAIR FOR PIPE INSTALLATION IS INCIDENTAL TO PIPE INSTALLATION.
- REFER TO MDOT SHA STD. NO. 578.03 FOR PERMANENT PATCHING FOR FLEXIBLE PAVEMENT USING APPROVED ASPHALT MIX.
- REFER TO MDOT SHA STD. NO. 580.03 FOR NEW CURB AND GUTTER PLACEMENT ALONG EXISTING PAVEMENT.
- REFER TO MDOT SHA STD. NO. 645.01 FOR STANDARD MONOLITHIC CONCRETE MEDIAN TYPE 'A'.
- THE SUBGRADE SHALL BE TEST ROLLED AS SPECIFIED IN SECTION 204 OF THE LATEST STANDARD SPECIFICATIONS FOR CONSTRUCTION AND MATERIALS.
- PROVIDE STEEL DOWELS 1.5" X 18", 1' ON CENTER, SIMILAR TO MDOT SHA TYPE 1 REPAIRS. PLACE DOWELS ALONG THE PERIMETER OF THE NEW CONSTRUCTION AND ALONG ANY EXISTING TRANSVERSE PAVEMENT JOINT.

DT-01

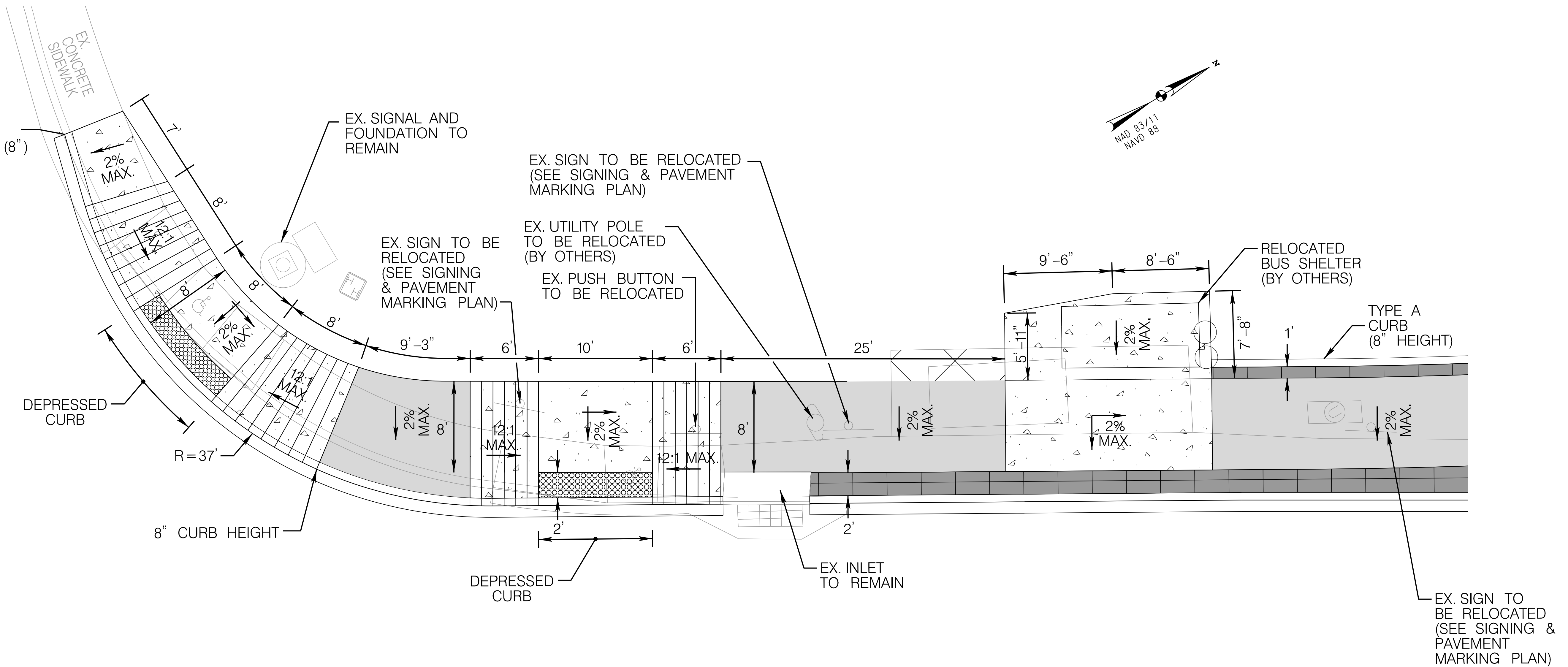
CITY OF TAKOMA PARK
NEW AVE BIKEWAY, SECTION B
MD 650 (NEW HAMPSHIRE AVENUE)
POPLAR AVE TO AUBURN AVE
SHA TRACKING NO. 20-AP-MO-020-XX

REVISIONS	PAVEMENT DETAILS	
60% PLANS NOVEMBER 2021	SCALE _N.T.S._ DATE _NOVEMBER 2021_ CONTRACT NO. _T.B.D._	
	DESIGNED BY _SAB_ COUNTY _MONTGOMERY_	
	DRAWN BY _BB_ LOGMILE _	
	CHECKED BY _R/JG_	
	F.A.P. NO. _T.B.D._	
	DRAWING NO. DT-01	1 OF 9 SHEET NO. 4 OF 44

PLOTTED: 11/3/2021
FILE: \\ad.rkk.com\its\Cloud\Projects\2020\20007_NewAveSecB\CADD\Plans\pDT-0001_NewAveBike_B.dgn

POPLAR AVE.

MATCH EX. CURB HEIGHT (8")



MD 650 SB (NEW HAMPSHIRE AVE.)

ADA AND BUS STOP DETAIL
STA.101+50,LT

NOTES:

- ALL CONCRETE CURB & GUTTER WILL BE PAID FOR AS STANDARD TYPE A COMBINATION CURB & GUTTER, ANY HEIGHT OR DEPTH.
- ALL CONCRETE CURB WILL BE PAID FOR AS STANDARD TYPE A CURB, ANY HEIGHT OR DEPTH, UNLESS NOTED OTHERWISE ON THE PLANS.
- IN AREAS OF CONCRETE SIDEWALK AND PAVEMENT REMOVAL, COMPLETELY REMOVE EXISTING PAVEMENT (OR SIDEWALK) TO THE DEFINED LIMITS OF CLASS 1 EXCAVATION. BACKFILL WITH FURNISHED SUBSOIL UP TO 4" BELOW FINAL GRADE, FOLLOWED BY 4" TOPSOIL AND TURFGRASS SOD ESTABLISHMENT.
- IN AREAS OF NEWLY PROPOSED CONCRETE SIDEWALK, TYPE 2 PAVEMENT, OR ASPHALT SHARED USE PATH, COMPLETELY REMOVE EXISTING PAVEMENT (OR SIDEWALK) TO THE DEFINED LIMITS OF CLASS 1 EXCAVATION. BACKFILL WITH COMMON BORROW TO THE BOTTOM OF SIDEWALK, TYPE 2 PAVEMENT OR SHARED USE PATH.

LEGEND

	5 INCH CONCRETE SIDEWALK
	CONCRETE DRIVEWAY
	SPECIALTY PAVER - TYPE 2
	ASPHALT SHARED USE PATH
	FULL DEPTH ASPHALT PAVING
	DETECTABLE WARNING SURFACE
	SIDEWALK AND PAVEMENT REMOVAL (PAID FOR AS CLASS 1 EXCAVATION)



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CITY OF TAKOMA PARK
NEW AVE BIKEWAY, SECTION B
MD 650 (NEW HAMPSHIRE AVENUE)
POPLAR AVE TO AUBURN AVE
SHA TRACKING NO. 20-AP-MO-020-XX

REVISIONS	ADA DETAILS		
60% PLANS NOVEMBER 2021	SCALE	1"=5'	DATE NOVEMBER 2021 CONTRACT NO. T.B.D.
	DESIGNED BY	KBJ	COUNTY MONTGOMERY
	DRAWN BY	SAB	
	CHECKED BY	RJG	
	F.A.P. NO.	T.B.D.	
	DRAWING NO. DT-02	2 OF 9	SHEET NO. 5 OF 44

PLOTTED: 11/2/2021
FILE: \\ad.rkk.com\its\Cloud\Projects\2020\20007_NewAveSecB\CADD\Plans\pDT-0002_NewAveBike_B.dgn



BY: kboampong -



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MD 650 SB (NEW HAMPSHIRE AVE.)

LEGEND

	5 INCH CONCRETE SIDEWALK
	CONCRETE DRIVEWAY
	SPECIALTY PAVER - TYPE 2
	ASPHALT SHARED USE PATH
	FULL DEPTH ASPHALT PAVING
	DETECTABLE WARNING SURFACE
	SIDEWALK AND PAVEMENT REMOVAL (PAID FOR AS CLASS 1 EXCAVATION)

NOTES:

- ALL CONCRETE CURB & GUTTER WILL BE PAID FOR AS STANDARD TYPE A COMBINATION CURB & GUTTER, ANY HEIGHT OR DEPTH.
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- IN AREAS OF NEWLY PROPOSED CONCRETE SIDEWALK, TYPE 2 PAVER OR ASPHALT SHARED USE PATH, COMPLETELY REMOVE EXISTING PAVEMENT (OR SIDEWALK) TO THE DEFINED LIMITS OF CLASS 1 EXCAVATION. BACKFILL WITH COMMON BORROW TO THE BOTTOM OF SIDEWALK, TYPE 2 PAVER OR SHARED USE PATH.

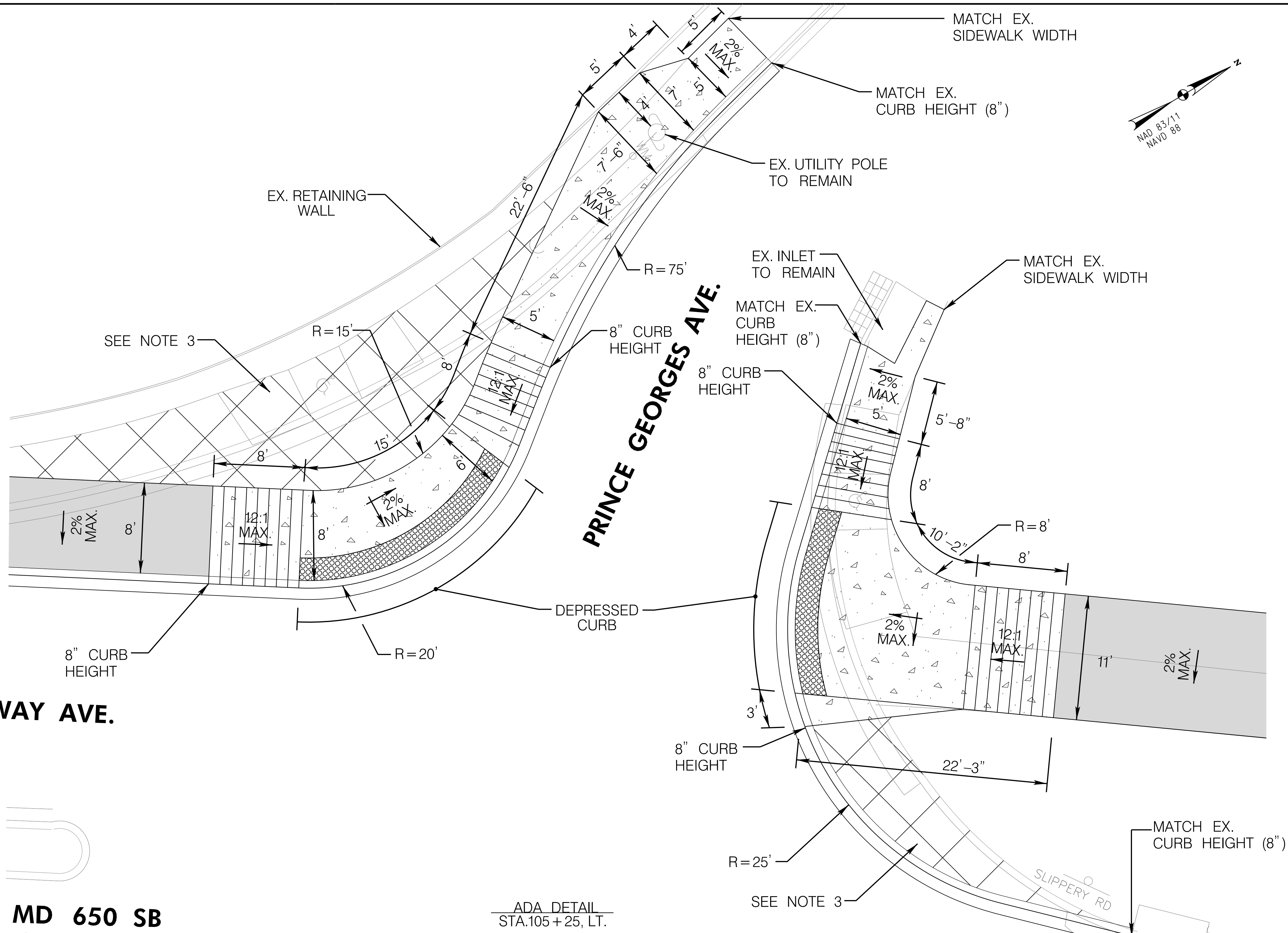
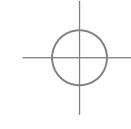
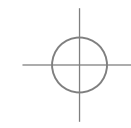
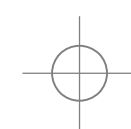
ADA DETAIL
STA.103 + 50,LT

DT-03

CITY OF TAKOMA PARK
NEW AVE BIKEWAY, SECTION B
MD 650 (NEW HAMPSHIRE AVENUE)
POPLAR AVE TO AUBURN AVE
SHA TRACKING NO. 20-AP-MO-020-XX

REVISIONS	ADA DETAILS
60% PLANS NOVEMBER 2021	SCALE 1"=5' DATE NOVEMBER 2021 CONTRACT NO. T.B.D.
	DESIGNED BY KBJ COUNTY MONTGOMERY
	DRAWN BY SAB
	CHECKED BY RJG
	F.A.P. NO. T.B.D.
	DRAWING NO. DT-03 3 OF 9 SHEET NO. 6 OF 44

PLOTTED: 11/2/2021
FILE: \\ad.rkk.com\its\Cloud\Projects\2020\20007_NewAveSecB\CADD\Plans\pDT-0003_NewAveBike_B.dgn



CONWAY AVE.

MD 650 SB
(NEW HAMPSHIRE AVE.)

ADA DETAIL
STA.105 + 25, LT.

NOTES:

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- IN AREAS OF CONCRETE SIDEWALK AND PAVEMENT REMOVAL, COMPLETELY REMOVE EXISTING PAVEMENT (OR SIDEWALK) TO THE DEFINED LIMITS OF CLASS 1 EXCAVATION, BACKFILL WITH FURNISHED SUBSOIL UP TO 4" BELOW FINAL GRADE, FOLLOWED BY 4" TOPSOIL AND TURFGRASS SOD ESTABLISHMENT.
- IN AREAS OF NEWLY PROPOSED CONCRETE SIDEWALK, TYPE 2 PAVEMENT, OR ASPHALT SHARED USE PATH, COMPLETELY REMOVE EXISTING PAVEMENT (OR SIDEWALK) TO THE DEFINED LIMITS OF CLASS 1 EXCAVATION, BACKFILL WITH COMMON BORROW TO THE BOTTOM OF SIDEWALK, TYPE 2 PAVEMENT OR SHARED USE PATH.

LEGEND

- | | |
|--|---|
| | 5 INCH CONCRETE SIDEWALK |
| | CONCRETE DRIVEWAY |
| | SPECIALTY PAVER - TYPE 2 |
| | ASPHALT SHARED USE PATH |
| | FULL DEPTH ASPHALT PAVING |
| | DETECTABLE WARNING SURFACE |
| | SIDEWALK AND PAVEMENT REMOVAL
(PAID FOR AS CLASS 1 EXCAVATION) |



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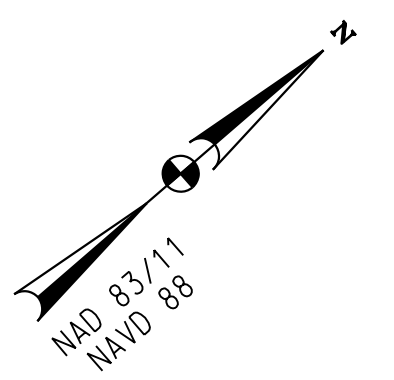
BY: kboampong -

DT-04

CITY OF TAKOMA PARK
NEW AVE BIKEWAY, SECTION B
MD 650 (NEW HAMPSHIRE AVENUE)
POPLAR AVE TO AUBURN AVE
SHA TRACKING NO. 20-AP-MO-020-XX

REVISIONS	ADA DETAILS	
60% PLANS NOVEMBER 2021	SCALE 1"=5' DATE NOVEMBER 2021 CONTRACT NO. T.B.D.	
	DESIGNED BY KBJ	COUNTY MONTGOMERY
	DRAWN BY SAB	
	CHECKED BY RJG	
F.A.P. NO. T.B.D.		
DRAWING NO. DT-04		SHEET NO. 7 OF 44

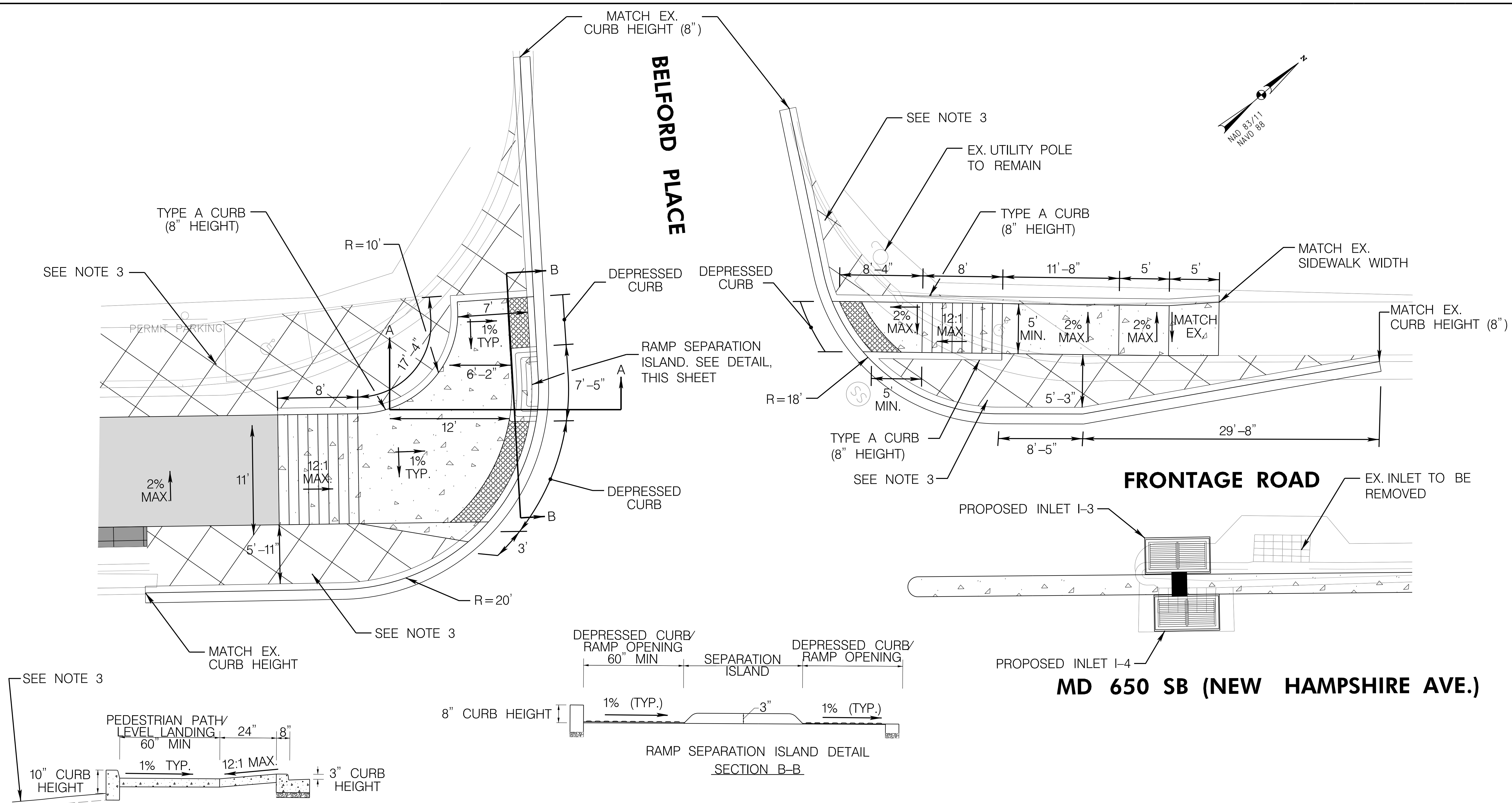
PLOTTED: 11/2/2021
FILE: \\ad.rkk.com\its\Cloud\Projects\2020\20007_NewAveSecB\CADD\Plans\pDT-0004_NewAveBike_B.dgn



BELFORD PLACE

FRONTAGE ROAD

MD 650 SB (NEW HAMPSHIRE AVE.)



RAMP SEPARATION ISLAND DETAIL
SECTION A-A

RAMP SEPARATION ISLAND DETAIL
SECTION B-B

ADA DETAIL
STA.109 + 60,LT

- NOTES:
- ALL CONCRETE CURB & GUTTER WILL BE PAID FOR AS STANDARD TYPE A COMBINATION CURB & GUTTER, ANY HEIGHT OR DEPTH.
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 - IN AREAS OF NEWLY PROPOSED CONCRETE SIDEWALK, TYPE 2 PAVER OR ASPHALT SHARED USE PATH, COMPLETELY REMOVE EXISTING PAVEMENT (OR SIDEWALK) TO THE DEFINED LIMITS OF CLASS 1 EXCAVATION. BACKFILL WITH COMMON BORROW TO THE BOTTOM OF SIDEWALK, TYPE 2 PAVER OR SHARED USE PATH.

LEGEND	
	5 INCH CONCRETE SIDEWALK
	CONCRETE DRIVEWAY
	SPECIALTY PAVER - TYPE 2
	ASPHALT SHARED USE PATH
	FULL DEPTH ASPHALT PAVING
	DETECTABLE WARNING SURFACE
	SIDEWALK AND PAVEMENT REMOVAL (PAID FOR AS CLASS 1 EXCAVATION)



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NEW AVE BIKEWAY, SECTION B
MD 650 (NEW HAMPSHIRE AVENUE)
POPLAR AVE TO AUBURN AVE
SHA TRACKING NO. 20-AP-MO-020-XX

REVISIONS		ADA DETAILS	
60% PLANS NOVEMBER 2021		SCALE 1"=5'	DATE NOVEMBER 2021 CONTRACT NO. T.B.D.
		DESIGNED BY KBJ	COUNTY MONTGOMERY
		DRAWN BY SAB	
		CHECKED BY RJG	
		F.A.P. NO. T.B.D.	
		DRAWING NO. DT-05	SHEET NO. 8 OF 44

PLOTTED: 11/2/2021
FILE: \\ad.rkk.com\ts\Cloud\Projects\2020\20007_NewAveSecB\CADD\Plans\pDT-0005_NewAveBike_B.dgn

BY: kboampong -

The diagram illustrates a plan view of a proposed intersection. Key features include:

- MATCH EX. CURB HEIGHT (6")**: Located at the top left corner.
- EX. TRAFFIC SIGNAL AND PUSH BUTTON TO BE RELOCATED (SEE TRAFFIC SIGNAL PLAN)**: Indicated by an arrow pointing to a circular feature near the top center.
- EX. BUS STOP BENCH TO BE REMOVED**: Indicated by an arrow pointing to a rectangular feature on the right side.
- PROPOSED PARKING WHEEL STOPS (SEE ROADWAY PLANS FOR ADDITIONAL INFORMATION)**: Indicated by arrows pointing to small circles on the far right.
- DEPRESSED CURB**: Labeled in two locations along the left boundary.
- TYPE A CURB (VARIES 6" TO 12" HEIGHT)**: Labeled twice, pointing to different curb sections.
- EX. SIGN TO BE RELOCATED (SEE SIGNING & PAVEMENT MARKING PLAN)**: Indicated by an arrow pointing to a sign symbol.
- EX. LIGHT POLE TO BE REMOVED**: Indicated by an arrow pointing to a pole symbol.
- EX. UTILITY POLE TO BE RELOCATED BY OTHERS**: Indicated by an arrow pointing to a pole symbol.
- Dimensions and Slopes**: Various measurements are provided, including radii ($R=5'$), lengths ($10'-7"$, $8'$, $15'-2"$), widths ($2' \pm$, $8'$, $6'$), and slopes ($2\% \text{ MAX}$, $12:1 \text{ MAX}$).
- Pavement Markings**: Includes a "BUS STOP" marking with an arrow and a dashed line.

ADA DETAIL
STA.115 + 50,LT

CITY OF TAKOMA PARK
NEW AVE BIKEWAY, SECTION B
MD 650 (NEW HAMPSHIRE AVENUE)
POPLAR AVE TO AUBURN AVE
SHA TRACKING NO. 20-AP-MO-020-XX

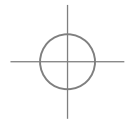
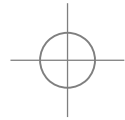
PLOTTED: 11/2/2021
FILE: \\ad.rkk.com\fs\Cloud\Projects\2020\20007 NewAveSecB\CADD\Plans\pDT-0006 NewAveBike B.dgn

1. ALL CONCRETE CURB & GUTTER WILL BE PAID FOR AS STANDARD TYPE A COMBINATION CURB & GUTTER, ANY HEIGHT OR DEPTH.
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4. IN AREAS OF NEWLY PROPOSED CONCRETE SIDEWALK, TYPE 2 PAVEMENT, OR ASPHALT SHARED USE PATH, COMPLETELY REMOVE EXISTING PAVEMENT (OR SIDEWALK) TO THE DEFINED LIMITS OF CLASS 1 EXCAVATION, BACKFILL WITH COMMON BORROW TO THE BOTTOM OF SIDEWALK, TYPE 2 PAVEMENT OR SHARED USE PATH.

	5 INCH CONCRETE SIDEWALK
	CONCRETE DRIVEWAY
	SPECIALTY PAVER - TYPE 2
	ASPHALT SHARED USE PATH
	FULL DEPTH ASPHALT PAVING
	DETECTABLE WARNING SURFACE
	SIDEWALK AND PAVEMENT REMOVAL (PAID FOR AS CLASS 1 EXCAVATION)



BY: kboampong -



BY: kboampong -



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	5 INCH CONCRETE SIDEWALK
	CONCRETE DRIVEWAY
	SPECIALTY PAVER - TYPE 2
	ASPHALT SHARED USE PATH
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ADA DETAIL
STA. 119 + 75, LT

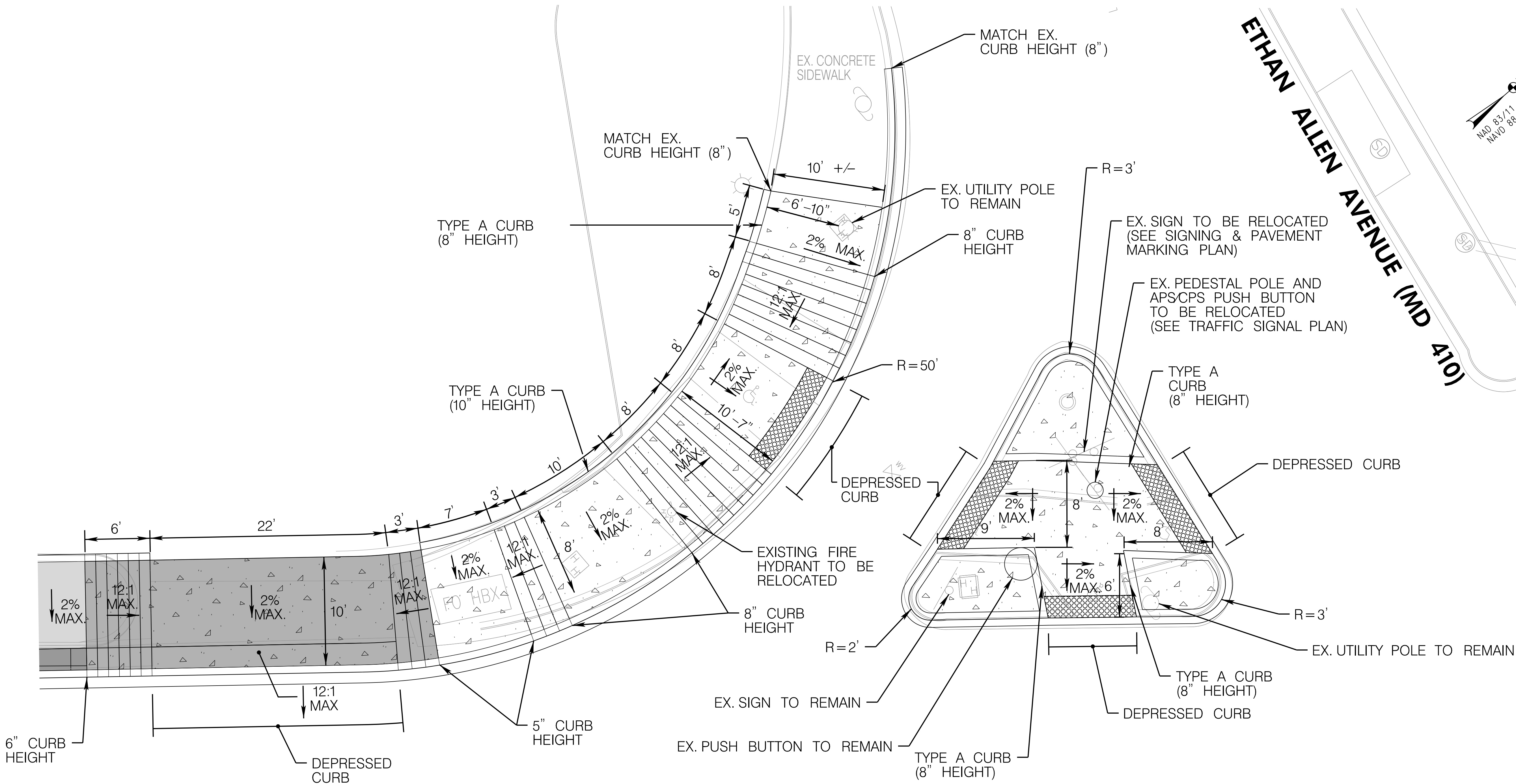
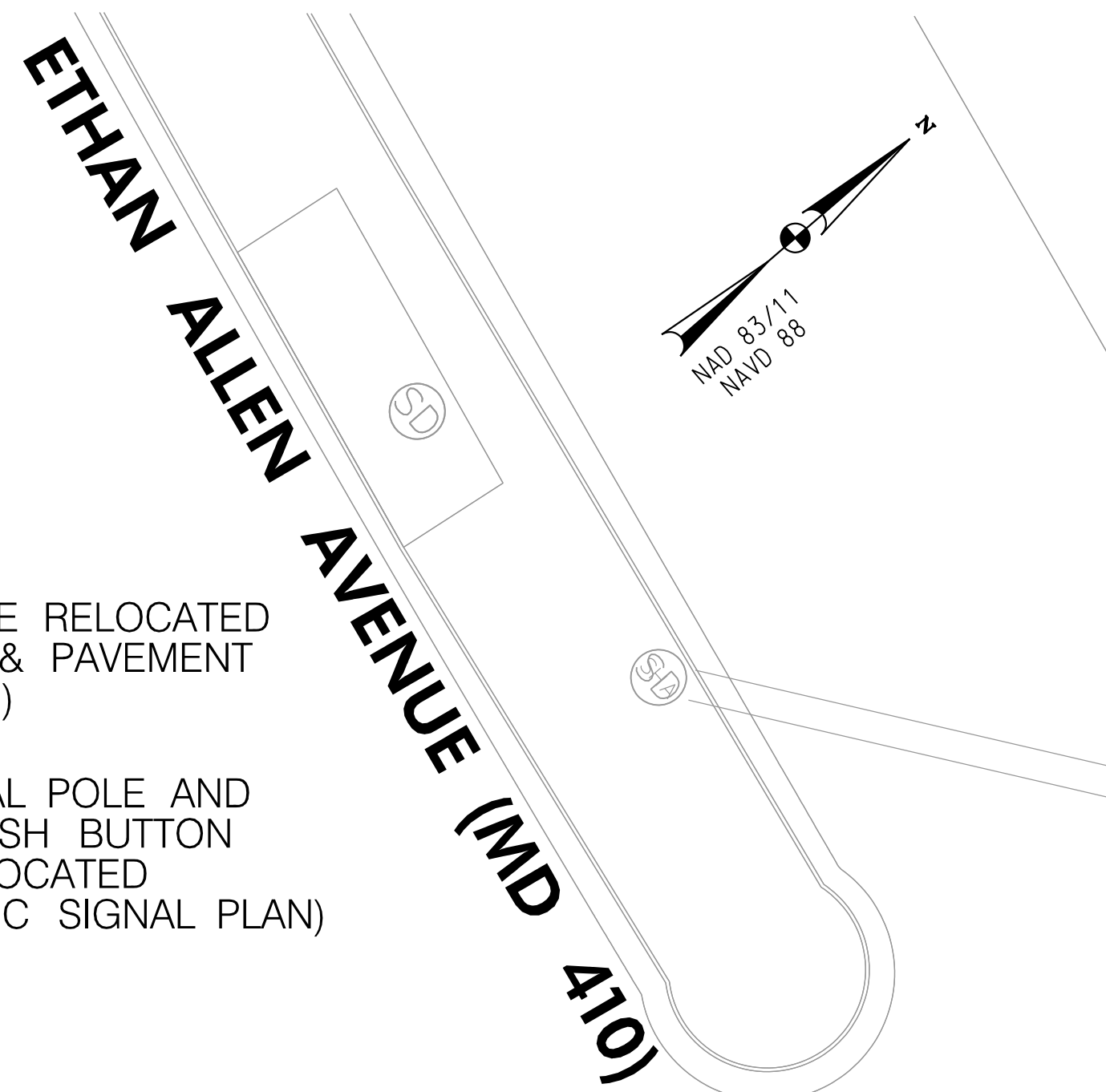
DT-07

CITY OF TAKOMA PARK
NEW AVE BIKEWAY, SECTION B
MD 650 (NEW HAMPSHIRE AVENUE)
POPLAR AVE TO AUBURN AVE
SHA TRACKING NO. 20-AP-MO-020-XX

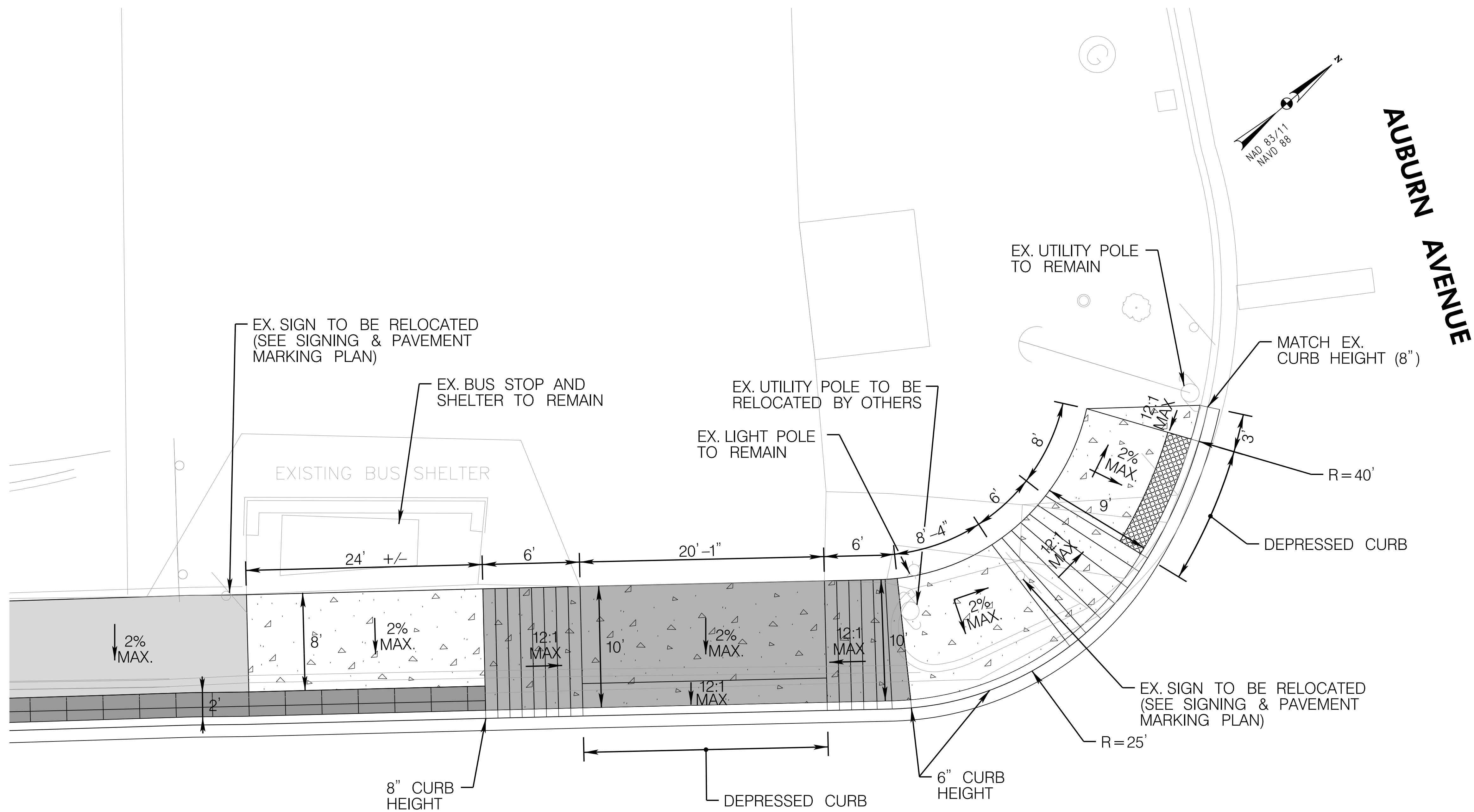
ADA DETAILS

SCALE 1"=5'	DATE NOVEMBER 2021	CONTRACT NO. T.B.D.
DESIGNED BY SAB / RJG	COUNTY MONTGOMERY	
DRAWN BY KBJ		
CHECKED BY RJG		
F.A.P. NO. T.B.D.		
DRAWING NO. DT-07	7 OF 9	SHEET NO. 10 OF 44

PLOTTED: 11/2/2021
FILE: \\ad.rkk.com\its\Cloud\Projects\2020\20007_NewAveSecB\CADD\Plans\pDT-0007_NewAveBike_B.dgn



MD 650 SB (NEW HAMPSHIRE AVE.)



MD 650 SB (NEW HAMPSHIRE AVE.)

ADA DETAIL
STA. 122 + 75, LT

DT-09

NOTES:

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LEGEND

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	DETECTABLE WARNING SURFACE
	SIDEWALK AND PAVEMENT REMOVAL (PAID FOR AS CLASS 1 EXCAVATION)



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CITY OF TAKOMA PARK
NEW AVE BIKEWAY, SECTION B
MD 650 (NEW HAMPSHIRE AVENUE)
POPLAR AVE TO AUBURN AVE
SHA TRACKING NO. 20-AP-MO-020-XX

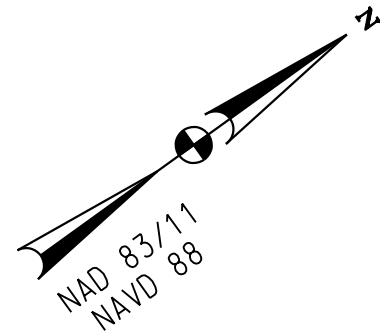
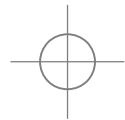
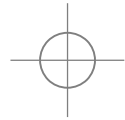
ADA DETAILS

SCALE 1"=5' DATE NOVEMBER 2021 CONTRACT NO. T.B.D.

DESIGNED BY KBJ / RJG COUNTY MONTGOMERY
DRAWN BY KBJ
CHECKED BY RJG
F.A.P. NO. T.B.D.

DRAWING NO. DT-09 9 OF 9 SHEET NO. 12 OF 44

PLOTTED: 11/2/2021
FILE: \\ad.rkk.com\its\Cloud\Projects\2020\20007_NewAveSecB\CADD\Plans\pDT-0009_NewAveBike_B.dgn

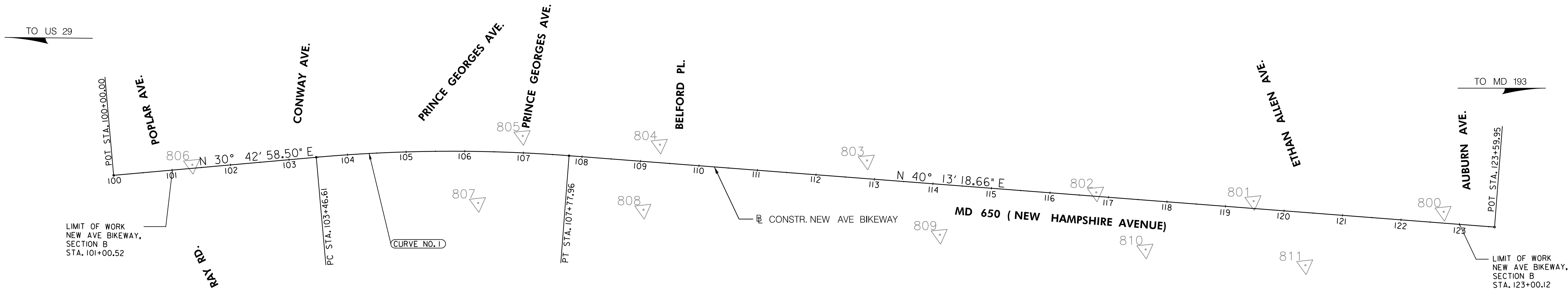


CURVE DATA						
CURVE NO.	Δ	Dc	R	T	L	E
I	9° 30' 20.16"	2° 12' 13.26"	2,600.00'	216.17'	431.35'	8.97'

BASELINE CONTROL COORDINATES CONSTR. MD 650		
	NORTH	EAST
POT STA. 100+00.00	474,968.1366	1,312,668.4796
PC STA. 103+46.61	475,266.1227	1,312,845.5252
PI STA. 105+62.78	475,451.9668	1,312,955.9426
PT STA. 107+77.96	475,617.0243	1,313,095.5350
POT STA. 123+59.95	476,824.9497	1,314,117.1010

TRAVERSE POINTS				
POINT NO.	NORTH	EAST	ELEVATION	PLAN SHEET NO.
659	477,089.7324	1,314,330.8027	178.33	-
800	476,770.0060	1,314,045.2907	189.90	PS-04
801	476,517.8914	1,313,841.0045	201.77	PS-04
802	476,309.1096	1,313,672.0181	211.04	PS-03
803	476,024.1202	1,313,399.3005	205.68	PS-03
804	475,753.7070	1,313,170.9315	176.51	PS-02
805	475,572.8268	1,313,021.9856	152.47	PS-02
806	475,087.4455	1,312,732.2320	136.99	PS-01
807	475,444.3321	1,313,070.1947	153.56	PS-02
808	475,666.1119	1,313,244.3769	176.67	PS-02
809	476,050.6859	1,313,574.5269	210.01	PS-03
810	476,320.3597	1,313,800.1156	208.26	PS-04
811	476,525.4742	1,313,981.2735	198.00	PS-04

NOTES:
1. TOPOGRAPHIC SURVEY AND BOUNDARY LINE ESTABLISHMENT
WAS PREPARED BY CAPITOL DEVELOPMENT DESIGN, INC. IN
MAY 2020.



GS-01

CITY OF TAKOMA PARK
NEW AVE BIKEWAY, SECTION B
MD 650 (NEW HAMPSHIRE AVENUE)
POPLAR AVE TO AUBURN AVE
SHA TRACKING NO. 20-AP-MO-020-XX

BASELINE GEOMETRY & SURVEY CONTROL

SCALE 1"=100' DATE NOVEMBER 2021 CONTRACT NO. T.B.D.

60% PLANS

NOVEMBER 2021

DESIGNED BY SAB COUNTY MONTGOMERY
DRAWN BY SAB LOGMILE
CHECKED BY RJG
F.A.P. NO. T.B.D.

DRAWING NO. GS-01 1 OF 5 SHEET NO. 13 OF 44



P: 410.728.2900 F: 410.728.2834
700 East Pratt Street, Suite 500 | Baltimore, MD 21202

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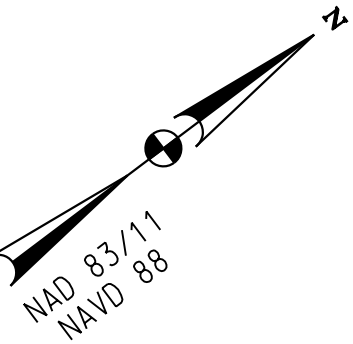
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BY: kboampong -

PLOTTED: 11/2/2021
FILE: \\ad.rkk.com\its\Cloud\Projects\2020\20007_NewAveSecB\CADD\Plans\pGS-0001_NewAveBike_B.dgn

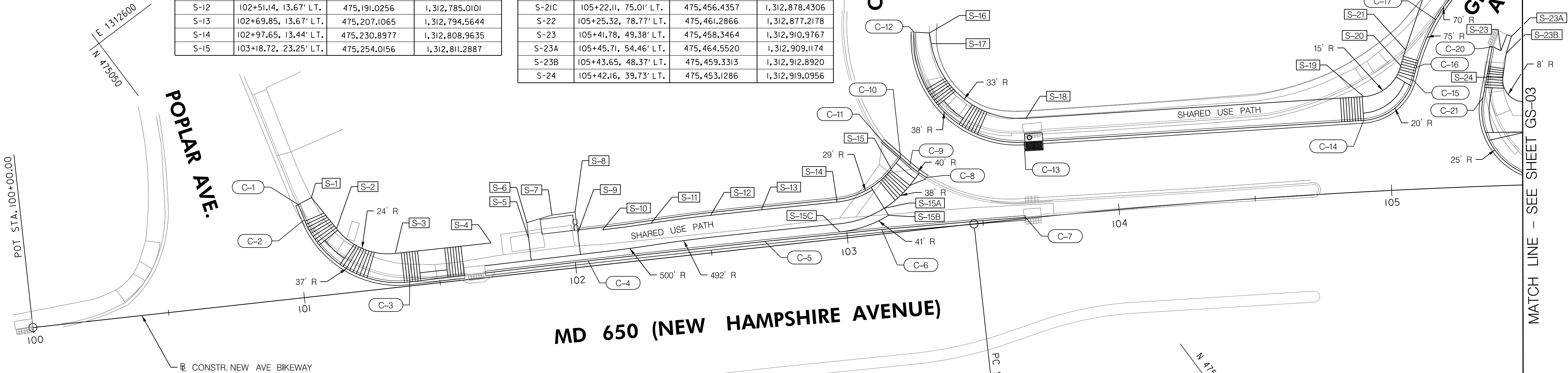
TO US 29

TO MD 193



SIDEWALK STAKEOUT CHART			
POINT NO.	STATION AND OFFSET	COORDINATES	
		NORTHING	EASTING
S-1	101+06.56, 36.13' LT.	475,078.2033	1,312,691.8542
S-2	101+14.46, 23.78' LT.	475,078.6833	1,312,706.4987
S-3	101+34.83, 12.43' LT.	475,090.4015	1,312,726.6623
S-4	101+70.13, 12.40' LT.	475,120.7302	1,312,744.7187
S-5	101+83.97, 12.51' LT.	475,132.6889	1,312,751.6993
S-6	101+83.93, 18.40' LT.	475,135.6596	1,312,746.6101
S-7	101+93.41, 20.11' LT.	475,144.6839	1,312,749.9850
S-8	102+01.91, 20.33' LT.	475,152.1045	1,312,754.1376
S-9	102+02.11, 12.65' LT.	475,148.3524	1,312,760.8425
S-10	102+11.22, 12.72' LT.	475,156.2218	1,312,765.4361
S-11	102+29.10, 13.18' LT.	475,171.8308	1,312,774.1709
S-12	102+51.14, 13.67' LT.	475,191.0256	1,312,785.0101
S-13	102+69.85, 13.67' LT.	475,207.1065	1,312,794.5644
S-14	102+97.65, 13.44' LT.	475,230.8977	1,312,808.9635
S-15	103+18.72, 23.25' LT.	475,254.0156	1,312,811.2887

SIDEWALK STAKEOUT CHART (CONT'D)			
POINT NO.	STATION AND OFFSET	COORDINATES	
		NORTHING	EASTING
S-15A	103+14.56, 9.29' LT.	475,243.3142	1,312,821.1700
S-15B	103+15.88, 6.59' LT.	475,243.0681	1,312,824.1599
S-15C	102+97.57, 2.44' LT.	475,225.2021	1,312,818.3741
S-16	103+38.03, 71.23' LT.	475,295.1262	1,312,779.8999
S-17	103+38.27, 67.28' LT.	475,293.3174	1,312,783.4215
S-18	103+69.38, 36.45' LT.	475,304.5375	1,312,826.0701
S-19	104+91.65, 34.79' LT.	475,408.0881	1,312,894.1778
S-20	105+05.56, 45.32' LT.	475,425.6957	1,312,893.3631
S-21	105+07.56, 51.12' LT.	475,430.6388	1,312,889.7122
S-21A	105+14.70, 69.77' LT.	475,447.1865	1,312,878.4383
S-21B	105+18.01, 73.43' LT.	475,452.0657	1,312,877.3457
S-21C	105+22.11, 75.01' LT.	475,456.4357	1,312,878.4306
S-22	105+25.32, 78.77' LT.	475,461.2866	1,312,877.2178
S-23	105+41.78, 49.38' LT.	475,458.3464	1,312,910.9767
S-23A	105+45.71, 54.46' LT.	475,464.5520	1,312,909.1174
S-23B	105+43.65, 48.37' LT.	475,459.3313	1,312,912.8920
S-24	105+42.16, 39.73' LT.	475,453.1286	1,312,919.0956



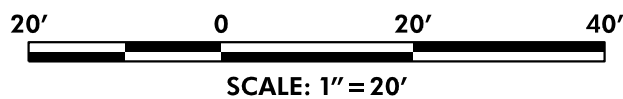
CURB STAKEOUT CHART					
POINT NO.	STATION AND OFFSET	EDGE OF ROAD	BOTTOM OF CURB	COORDINATES	
				NORTHING	EASTING
C-1	101+01.46, 34.09' LT.	136.25	136.21	475,072.7794	1,312,690.9947
C-2	101+03.10, 28.35' LT.	136.71	136.67	475,071.2533	1,312,696.7641
C-3	101+38.97, 1.49' LT.	136.34	136.30	475,088.3704	1,312,738.1805
C-4	102+04.72, 2.00' LT.	136.54	136.50	475,145.1553	1,312,771.3272
C-5	102+69.80, 2.00' LT.	137.38	137.34	475,201.1064	1,312,804.5700
C-6	103+12.82, 1.65' LT.	137.97	137.93	475,237.9117	1,312,826.8456
C-7	103+65.59, 0.39' LT.	139.07	139.03	475,282.6000	1,312,854.9426
C-8	103+31.20, 18.79' LT.	140.41	140.37	475,262.4654	1,312,821.4983
C-9	103+25.62, 23.84' LT.	140.64	140.60	475,260.2507	1,312,814.3078
C-10	103+22.55, 27.20' LT.	140.88	140.84	475,259.3257	1,312,809.8498
C-11	103+16.57, 33.74' LT.	141.33	141.29	475,257.5256	1,312,801.1745
C-12	103+32.41, 72.18' LT.	MATCH EX.	MATCH EX.	475,290.7772	1,312,776.2172
C-13	103+67.89, 27.83' LT.	139.21	139.17	475,298.7805	1,312,832.6568
C-14	104+91.20, 26.11' LT.	143.83	143.79	475,402.8714	1,312,901.1199
C-15	105+09.72, 39.96' LT.	144.32	144.28	475,426.1668	1,312,900.1768

CURB STAKEOUT CHART (CONT'D)					
POINT NO.	STATION AND OFFSET	EDGE OF ROAD	BOTTOM OF CURB	COORDINATES	
				NORTHING	EASTING
C-16	105+11.14, 44.48' LT.	144.30	144.26	475,429.9135	1,312,897.2527
C-17	105+18.92, 61.62' LT.	144.21	144.17	475,446.1445	1,312,887.6009
C-18	105+26.02, 71.33' LT.	MATCH EX.	MATCH EX.	475,457.6519	1,312,883.7433
C-19	105+29.29, 75.11' LT.	MATCH EX.	MATCH EX.	475,462.5696	1,312,882.5573
C-20	105+38.71, 50.92' LT.	143.87	143.83	475,456.6679	1,312,907.9232
C-21	105+34.70, 32.46' LT.	145.04	145.00	475,442.7600	1,312,920.7265

NOTES:

- STAKEOUT POINT NUMBERS REFERENCE THE FACE OF PROPOSED CURB OR THE BACK OF PROPOSED SIDEWALK.
- RADII REFERENCE THE FACE OF PROPOSED CURB.
- ALL PROPOSED SIDEWALK SHALL HAVE A 2% (MAX) CROSS SLOPE. SEE ADA DETAILS FOR PEDESTRIAN RAMP DESIGN.

N 47500
E 1312950



CITY OF TAKOMA PARK
NEW AVE BIKEWAY, SECTION B
MD 650 (NEW HAMPSHIRE AVENUE)
POPLAR AVE TO AUBURN AVE
SHA TRACKING NO. 20-AP-MO-020-XX

STAKEOUT DETAIL

SCALE 1"=20' DATE NOVEMBER 2021 CONTRACT NO. T.B.D.

DESIGNED BY SAB COUNTY MONTGOMERY
DRAWN BY KBJ LOGMILE
CHECKED BY RJG
F.A.P. NO. T.B.D.

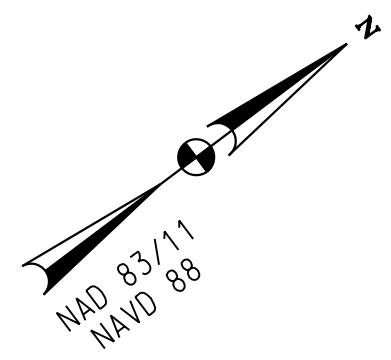
DRAWING NO. GS-02 2 OF 5 SHEET NO. 14 OF 44

60% PLANS
NOVEMBER 2021

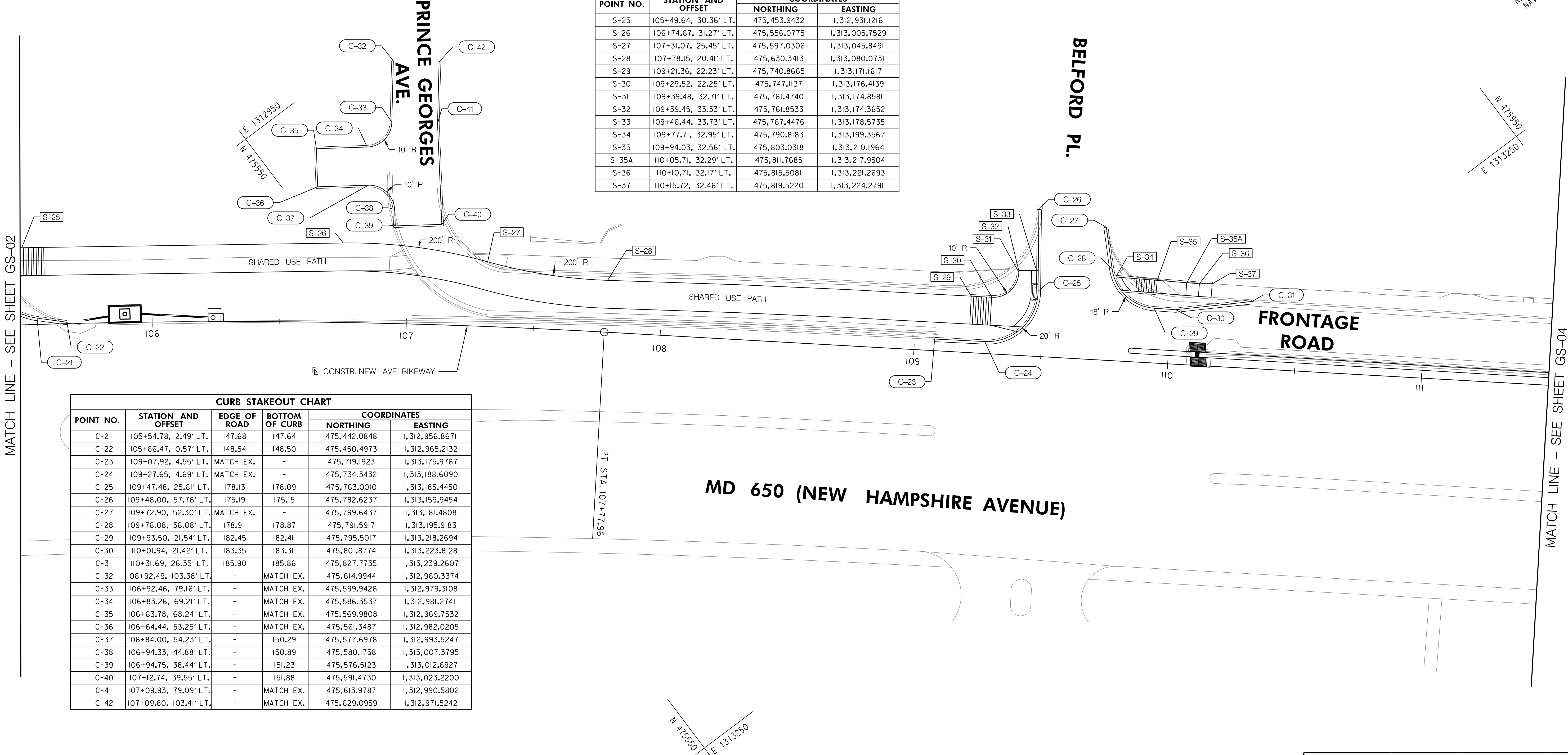
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TO US 29

TO MD 193



SIDEWALK STAKEOUT CHART			
POINT NO.	STATION AND OFFSET	COORDINATES	
		NORTHING	EASTING
S-25	105+49.64, 30.36' LT.	475,453.9432	1,312,931.1216
S-26	106+74.67, 31.27' LT.	475,556.0775	1,313,005.7529
S-27	107+31.07, 25.45' LT.	475,597.0306	1,313,045.8491
S-28	107+78.15, 20.41' LT.	475,630.3413	1,313,080.0731
S-29	109+21.36, 22.23' LT.	475,740.8665	1,313,171.1617
S-30	109+29.52, 22.25' LT.	475,747.1137	1,313,176.4139
S-31	109+39.48, 32.71' LT.	475,761.4740	1,313,174.8581
S-32	109+39.45, 33.33' LT.	475,761.8533	1,313,174.3652
S-33	109+46.44, 33.73' LT.	475,767.4476	1,313,178.5735
S-34	109+77.71, 32.95' LT.	475,790.8183	1,313,199.3567
S-35	109+94.03, 32.56' LT.	475,803.0318	1,313,210.1964
S-35A	110+05.71, 32.29' LT.	475,811.7685	1,313,217.9504
S-36	110+10.71, 32.17' LT.	475,815.5081	1,313,221.2693
S-37	110+15.72, 32.46' LT.	475,819.5220	1,313,224.2791



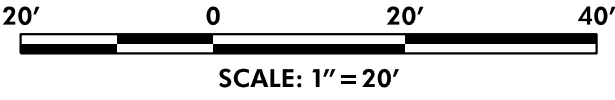
CURB STAKEOUT CHART					
POINT NO.	STATION AND OFFSET	EDGE OF ROAD	BOTTOM OF CURB	COORDINATES	
				NORTHING	EASTING
C-21	105+54.78, 2.49' LT.	147.68	147.64	475,442.0848	1,312,956.8671
C-22	105+66.47, 0.57' LT.	148.54	148.50	475,450.4973	1,312,965.2132
C-23	109+07.92, 4.55' LT.	MATCH EX.	-	475,719.1923	1,313,175.9767
C-24	109+27.65, 4.69' LT.	MATCH EX.	-	475,734.3432	1,313,188.6090
C-25	109+47.48, 25.61' LT.	178.13	178.09	475,763.0010	1,313,185.4450
C-26	109+46.00, 57.76' LT.	175.19	175.15	475,782.6237	1,313,159.9454
C-27	109+72.90, 52.30' LT.	MATCH EX.	-	475,799.6437	1,313,181.4808
C-28	109+76.08, 36.08' LT.	178.91	178.87	475,791.5917	1,313,195.9183
C-29	109+93.50, 21.54' LT.	182.45	182.41	475,795.5017	1,313,218.2694
C-30	110+01.94, 21.42' LT.	183.35	183.31	475,801.8774	1,313,223.8128
C-31	110+31.69, 26.35' LT.	185.90	185.86	475,827.7735	1,313,239.2607
C-32	106+92.49, 103.38' LT.	-	MATCH EX.	475,614.9944	1,312,960.3374
C-33	106+92.46, 79.16' LT.	-	MATCH EX.	475,599.9426	1,312,979.3108
C-34	106+83.26, 69.21' LT.	-	MATCH EX.	475,586.3537	1,312,981.2741
C-35	106+63.78, 68.24' LT.	-	MATCH EX.	475,569.9808	1,312,969.7532
C-36	106+64.44, 53.25' LT.	-	MATCH EX.	475,561.3487	1,312,982.0205
C-37	106+84.00, 54.23' LT.	-	150.29	475,577.6978	1,312,993.5247
C-38	106+94.33, 44.88' LT.	-	150.89	475,580.1758	1,313,007.3795
C-39	106+94.75, 38.44' LT.	-	151.23	475,576.5123	1,313,012.6927
C-40	107+12.74, 39.55' LT.	-	151.88	475,591.4730	1,313,023.2200
C-41	107+09.93, 79.09' LT.	-	MATCH EX.	475,613.9787	1,312,990.5802
C-42	107+09.80, 103.41' LT.	-	MATCH EX.	475,629.0959	1,312,971.5242

- NOTES:
- STAKEOUT POINT NUMBERS REFERENCE THE FACE OF PROPOSED CURB OR THE BACK OF PROPOSED SIDEWALK.
 - RADIREFERENCE THE FACE OF PROPOSED CURB.
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CITY OF TAKOMA PARK
NEW AVE BIKEWAY, SECTION B
MD 650 (NEW HAMPSHIRE AVENUE)
POPLAR AVE TO AUBURN AVE
SHA TRACKING NO. 20-AP-MO-020-XX

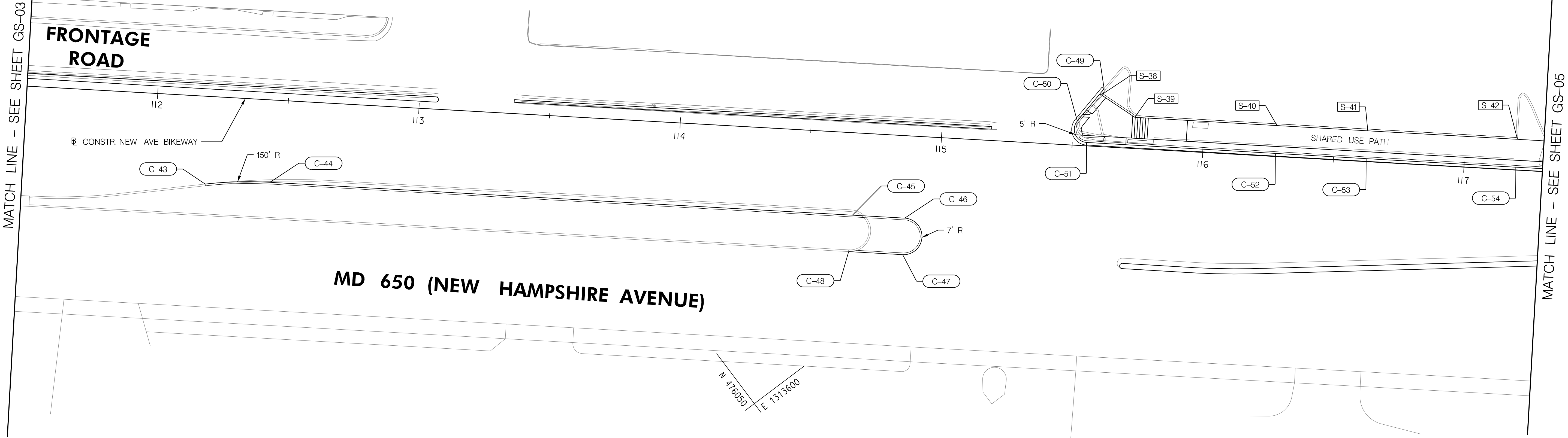
STAKEOUT DETAIL

SCALE 1"=20' DATE NOVEMBER 2021 CONTRACT NO. T.B.D.	
DESIGNED BY SAB	COUNTY MONTGOMERY
DRAWN BY KBJ	LOGMILE
CHECKED BY RJG	
F.A.P. NO. T.B.D.	
DRAWING NO. GS-03	3 OF 5 SHEET NO. 15 OF 44

60% PLANS
NOVEMBER 2021

CURB STAKEOUT CHART					
POINT NO.	STATION AND OFFSET	EDGE OF ROAD	BOTTOM OF CURB	COORDINATES	
				NORTHING	EASTING
C-43	112+20.04, 33.55' RT.	MATCH EX.	-	475,932.9077	1,313,406.6183
C-44	112+44.73, 31.50' RT.	MATCH EX.	-	475,953.0823	1,313,406.6183
C-45	114+67.70, 31.50' RT.	MATCH EX.	-	476,123.3327	1,313,564.9845
C-46	114+87.70, 31.50' RT.	MATCH EX.	-	476,138.6037	1,313,577.8995
C-47	114+87.70, 45.50' RT.	MATCH EX.	-	476,129.5632	1,313,588.5892
C-48	114+67.03, 45.50' RT.	MATCH EX.	-	476,113.7797	1,313,575.2408
C-49	115+60.20, 22.75' LT.	209.93	209.89	476,228.9911	1,313,583.2884
C-50	115+51.47, 9.27' LT.	210.15	210.18	476,213.6242	1,313,587.9469
C-51	115+55.48, 1.30' LT.	MATCH EX.	-	476,211.5391	1,313,596.6254
C-52	116+27.74, 1.18' LT.	MATCH EX.	-	476,266.6370	1,313,643.3767
C-53	116+62.51, 1.13' LT.	MATCH EX.	-	476,293.1438	1,313,665.8681
C-54	117+19.80, 1.09' LT.	MATCH EX.	-	476,336.8689	1,313,702.8890

SIDEWALK STAKEOUT CHART			
POINT NO.	STATION AND OFFSET	COORDINATES	
		NORTHING	EASTING
S-38	115+60.08, 19.79' LT.	476,226.9840	1,313,585.4752
S-39	115+73.32, 11.94' LT.	476,232.0239	1,313,600.0179
S-40	116+27.80, 11.85' LT.	476,273.5655	1,313,635.2665
S-41	116+62.52, 11.79' LT.	476,300.0407	1,313,657.7311
S-42	117+19.80, 11.76' LT.	476,343.7595	1,313,694.7467



- NOTES:
- STAKEOUT POINT NUMBERS REFERENCE THE FACE OF PROPOSED CURB OR THE BACK OF PROPOSED SIDEWALK.
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CITY OF TAKOMA PARK
NEW AVE BIKEWAY, SECTION B
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POPLAR AVE TO AUBURN AVE
SHA TRACKING NO. 20-AP-MO-020-XX

STAKEOUT DETAIL

SCALE 1"=20' DATE NOVEMBER 2021 CONTRACT NO. T.B.D.

DESIGNED BY SAB COUNTY MONTGOMERY
DRAWN BY KBJ LOGMILE
CHECKED BY RJG
F.A.P. NO. T.B.D.

DRAWING NO. GS-04 4 OF 5 SHEET NO. 16 OF 44

60% PLANS
NOVEMBER 2021

20' 0 20' 40'
SCALE: 1"=20'

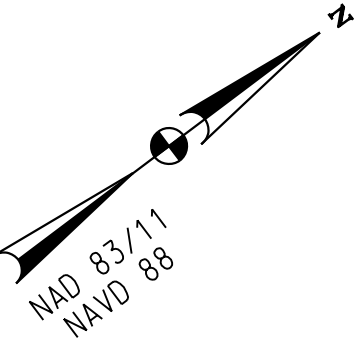
PLOTTED: 11/2/2021
FILE: \\ad.rkk.com\its\Cloud\Projects\2020\20007_NewAveSecB\CADD\Plans\pGS-0004_NewAveBike_B.dgn

TO US 29

TO MD 193

E 1313600
N 476500

N 476850
E 1313950



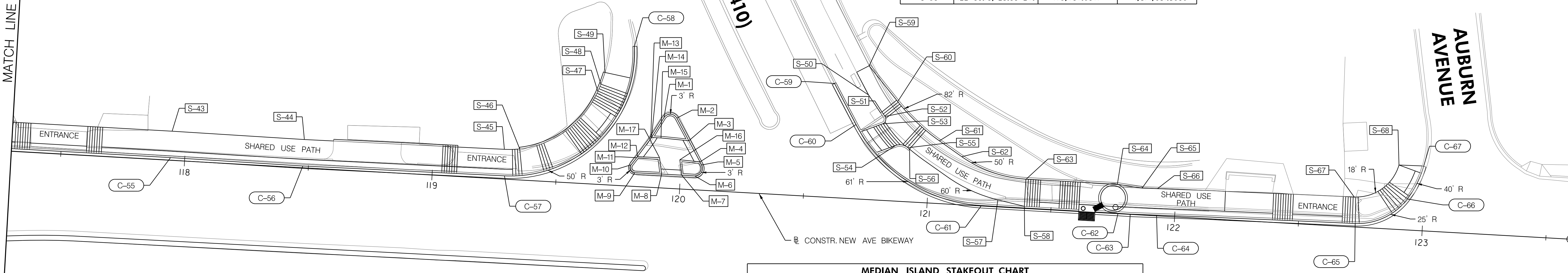
CURB STAKEOUT CHART					
POINT NO.	STATION AND OFFSET	EDGE OF ROAD	BOTTOM OF CURB	COORDINATES	
				NORTHING	EASTING
C-55	117+94.27, 1.09' LT.	MATCH EX.	MATCH EX.	476,393.7232	1,313,750.9809
C-56	118+47.75, 0.71' LT.	MATCH EX.	MATCH EX.	476,434.3180	1,313,785.8079
C-57	119+29.17, 1.32' LT.	MATCH EX.	MATCH EX.	476,496.8796	1,313,837.9188
C-58	119+78.54, 56.41' LT.	199.02	198.98	476,570.1467	1,313,827.7332
C-59	120+60.03, 46.01' LT.	196.81	196.77	476,625.6535	1,313,888.2973
C-60	120+69.59, 29.96' LT.	196.55	196.51	476,622.5892	1,313,906.7225
C-61	121+21.96, 0.18' LT.	MATCH EX.	MATCH EX.	476,643.3452	1,313,963.2836
C-62	121+77.13, 0.14' LT.	MATCH EX.	MATCH EX.	476,685.4487	1,313,998.9330
C-63	121+82.06, 0.10' LT.	MATCH EX.	MATCH EX.	476,689.1866	1,314,002.1583
C-64	121+92.68, 0.31' LT.	MATCH EX.	MATCH EX.	476,697.4296	1,314,008.8529
C-65	122+72.86, 1.44' LT.	MATCH EX.	MATCH EX.	476,759.3869	1,314,059.7646
C-66	122+93.31, 12.48' LT.	MATCH EX.	MATCH EX.	476,782.1320	1,314,064.5388
C-67	122+99.15, 26.01' LT.	MATCH EX.	MATCH EX.	476,795.3200	1,314,057.9767

SIDEWALK STAKEOUT CHART			
POINT NO.	STATION AND OFFSET	COORDINATES	
		NORTHING	EASTING
S-43	117+94.30, 11.75' LT.	476,400.6404	1,313,742.8610
S-44	118+47.75, 11.38' LT.	476,441.2045	1,313,777.6617
S-45	119+28.55, 11.98' LT.	476,503.2888	1,313,829.3751
S-46	119+34.72, 12.52' LT.	476,508.3503	1,313,832.9503
S-47	119+64.84, 37.47' LT.	476,547.4595	1,313,833.3495
S-48	119+65.93, 40.42' LT.	476,550.1930	1,313,831.8023
S-49	119+67.31, 45.22' LT.	476,554.3485	1,313,829.0234
S-50	120+73.97, 42.16' LT.	476,633.8173	1,313,900.2424
S-51	120+79.76, 35.61' LT.	476,634.0050	1,313,908.9724
S-52	120+81.07, 33.88' LT.	476,633.8855	1,313,911.1460
S-53	120+80.26, 30.83' LT.	476,631.3010	1,313,912.9479
S-54	120+86.02, 22.42' LT.	476,630.2673	1,313,923.0918
S-55	120+91.02, 21.79' LT.	476,633.6833	1,313,926.8037
S-56	120+91.71, 21.27' LT.	476,633.8680	1,313,927.6487
S-57	121+28.43, 2.84' LT.	476,650.0089	1,313,965.4270
S-58	121+39.25, 0.83' LT.	476,656.9692	1,313,973.9456
S-59	120+73.54, 53.92' LT.	476,641.0799	1,313,890.9807
S-60	120+85.85, 40.16' LT.	476,641.5937	1,313,909.4421
S-61	121+03.54, 25.27' LT.	476,645.4877	1,313,932.2292
S-62	121+12.91, 19.42' LT.	476,648.8655	1,313,942.7492
S-63	121+39.36, 11.83' LT.	476,664.1623	1,313,965.6226
S-64	121+76.19, 11.81' LT.	476,692.2640	1,313,989.4166
S-65	121+86.25, 10.85' LT.	476,699.3274	1,313,996.6528
S-66	121+92.47, 10.97' LT.	476,704.1558	1,314,000.5743
S-67	122+71.88, 12.10' LT.	476,765.5125	1,314,050.9895
S-68	122+89.19, 25.98' LT.	476,787.6971	1,314,051.5686

MATCH LINE - SEE SHEET GS-04

ETHAN ALLEN AVENUE (MD 410)

AUBURN AVENUE



MD 650 (NEW HAMPSHIRE AVENUE)

N 476500
E 1313950

MEDIAN ISLAND STAKEOUT CHART					
POINT NO.	STATION AND OFFSET	EDGE OF ROAD	BOTTOM OF CURB	COORDINATES	
				NORTHING	EASTING
M-1	119+92.07, 28.35' LT.	199.28	199.24	476,562.3683	1,313,857.8953
M-2	119+97.27, 28.46' LT.	199.11	199.07	476,566.4064	1,313,861.1675
M-3	120+02.63, 19.61' LT.	199.09	199.05	476,564.7804	1,313,871.3834
M-4	120+07.61, 11.38' LT.	199.05	199.01	476,563.2671	1,313,880.8912
M-5	120+08.34, 10.16' LT.	198.96	198.92	476,563.0444	1,313,882.2900
M-6	120+05.77, 5.61' LT.	199.21	199.17	476,558.1420	1,313,884.1070
M-7	120+00.55, 5.62' LT.	199.43	199.39	476,554.1554	1,313,880.7281
M-8	119+92.41, 5.60' LT.	199.77	199.73	476,547.9365	1,313,875.4869
M-9	119+81.61, 5.52' LT.	200.19	200.15	476,539.6331	1,313,868.5715
M-10	119+79.95, 8.66' LT.	200.16	200.12	476,540.3896	1,313,865.1044
M-11	119+81.98, 11.61' LT.	200.05	200.01	476,543.8468	1,313,864.1669
M-12	119+82.28, 12.06' LT.	200.02	199.98	476,544.3708	1,313,864.0138
M-13	119+87.24, 19.56' LT.	199.82	199.78	476,553.0006	1,313,861.4923
M-14	119+87.54, 20.10' LT.	199.79	199.75	476,553.5778	1,313,861.2707
M-15	119+91.36, 20.13' LT.	-	200.38	476,556.5129	1,313,863.7151
M-16	119+99.41, 11.75' LT.	-	199.57	476,557.2512	1,313,875.3114
M-17	119+91.21, 12.13' LT.	-	199.91	476,551.2292	1,313,869.7258

NOTES:

- STAKEOUT POINT NUMBERS REFERENCE THE FACE OF PROPOSED CURB OR THE BACK OF PROPOSED SIDEWALK.
- RADII REFERENCE THE FACE OF PROPOSED CURB.
- ALL PROPOSED SIDEWALK SHALL HAVE A 2% (MAX) CROSS SLOPE. SEE ADA DETAILS FOR PEDESTRIAN RAMP DESIGN.



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CITY OF TAKOMA PARK
NEW AVE BIKEWAY, SECTION B
MD 650 (NEW HAMPSHIRE AVENUE)
POPLAR AVE TO AUBURN AVE
SHA TRACKING NO. 20-AP-MO-020-XX

STAKEOUT DETAIL

SCALE 1"=20' DATE NOVEMBER 2021 CONTRACT NO. T.B.D.

DESIGNED BY SAB COUNTY MONTGOMERY
DRAWN BY KBJ LOGMILE
CHECKED BY RJG
F.A.P. NO. T.B.D.

DRAWING NO. GS-05 5 OF 5 SHEET NO. 17 OF 44

60% PLANS

NOVEMBER 2021

20' 0 20' 40'
SCALE: 1"=20'

PLOTTED: 11/2/2021
FILE: \\ad.rkk.com\its\Cloud\Projects\2020\20007_NewAveSecB\CADD\Plans\pGS-0005_NewAveBike_B.dgn

LIMIT OF WORK
CONTR. NO. T.B.D.
NEW AVE BIKEWAY
STA. 101+00.52

5 INCH CONCRETE SIDEWALK		
STA. 101+02.09 TO STA. 101+27.27, LT		264 SF
STA. 101+37.04 TO STA. 101+59.03, LT		224 SF
STA. 101+84.04 TO STA. 102+02.17, LT		273 SF
STA. 103+10.62 TO STA. 103+30.80, LT		154 SF
STA. 103+33.06 TO STA. 103+55.67, LT		313 SF
STA. 104+83.22 TO STA. 105+28.81, LT		428 SF
STA. 105+34.78 TO STA. 105+50.00, LT		312 SF

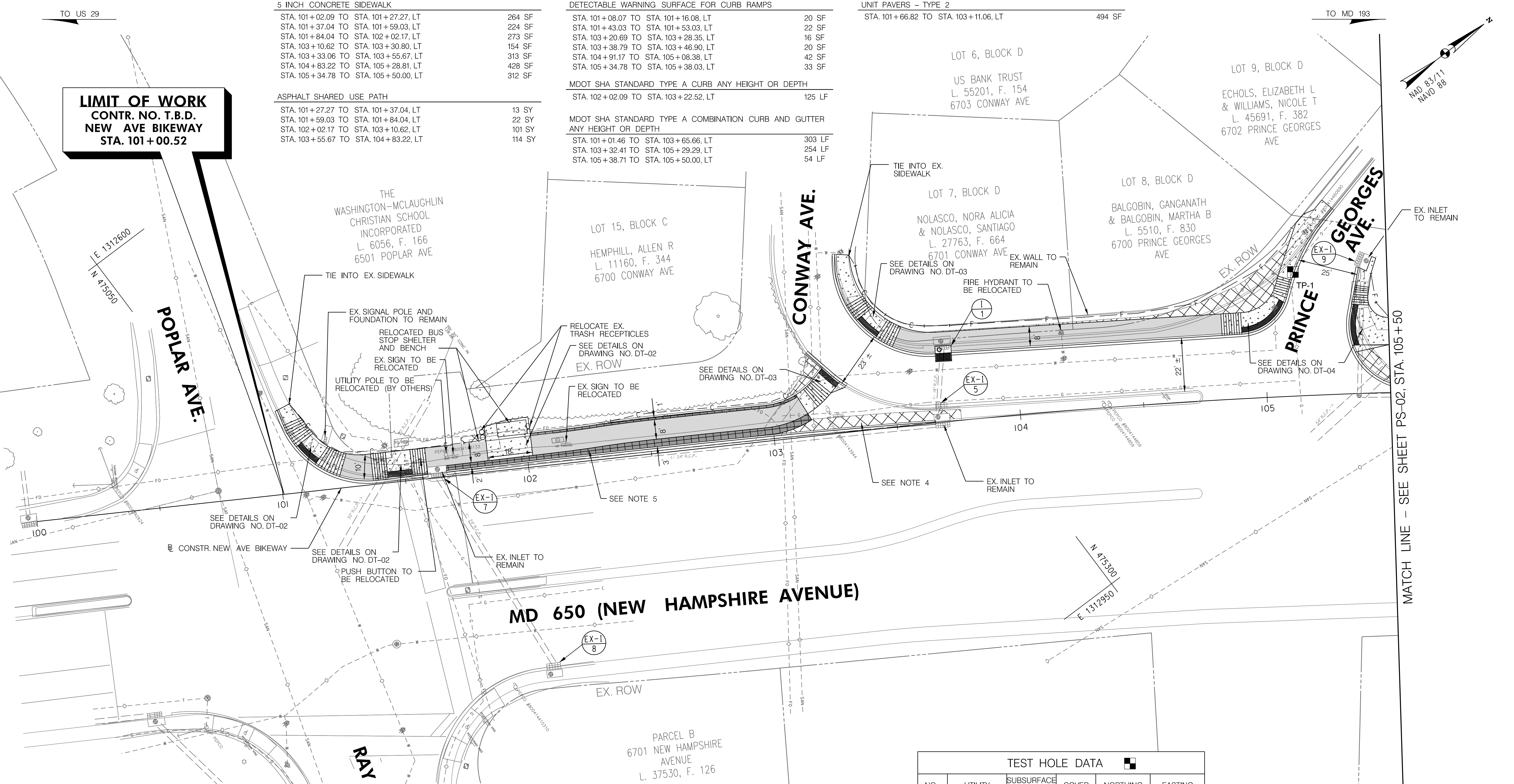
ASPHALT SHARED USE PATH		
STA. 101+27.27 TO STA. 101+37.04, LT		13 SY
STA. 101+59.03 TO STA. 101+84.04, LT		22 SY
STA. 102+02.17 TO STA. 103+10.62, LT		101 SY
STA. 103+55.67 TO STA. 104+83.22, LT		114 SY

DETECTABLE WARNING SURFACE FOR CURB RAMPS		
STA. 101+08.07 TO STA. 101+16.08, LT		20 SF
STA. 101+43.03 TO STA. 101+53.03, LT		22 SF
STA. 103+20.69 TO STA. 103+28.35, LT		16 SF
STA. 103+38.79 TO STA. 103+46.90, LT		20 SF
STA. 104+91.17 TO STA. 105+08.38, LT		42 SF
STA. 105+34.78 TO STA. 105+38.03, LT		33 SF

MDOT SHA STANDARD TYPE A CURB ANY HEIGHT OR DEPTH		
STA. 102+02.09 TO STA. 103+22.52, LT		125 LF

MDOT SHA STANDARD TYPE A COMBINATION CURB AND GUTTER ANY HEIGHT OR DEPTH		
STA. 101+01.46 TO STA. 103+65.66, LT		303 LF
STA. 103+32.41 TO STA. 105+29.29, LT		254 LF
STA. 105+38.71 TO STA. 105+50.00, LT		54 LF

UNIT PAVERS - TYPE 2		
STA. 101+66.82 TO STA. 103+11.06, LT		494 SF

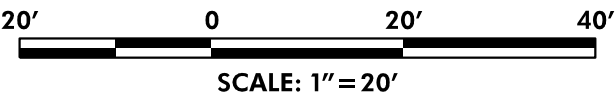


MD 650 (NEW HAMPSHIRE AVENUE)

TEST HOLE DATA					
NO.	UTILITY	SUBSURFACE ELEVATION	COVER	NORTHING	EASTING
TP-1	GAS	140.02'	4.72'	475,475.3671	1,312,890.5783

NOTES

- ALL CONCRETE CURB AND GUTTER WILL BE PAID FOR AS STANDARD TYPE A COMBINATION CURB & GUTTER, ANY HEIGHT OR DEPTH.
- ALL CONCRETE CURB WILL BE PAID FOR AS STANDARD TYPE A CURB, ANY HEIGHT OR DEPTH (EXCEPTION BEING TYPE B CURB FOR MD 650 MEDIAN WORK).
- SEE TRAFFIC SIGNAL PLANS FOR IMPACTS TO EXISTING TRAFFIC SIGNAL EQUIPMENT AND PROPOSED MODIFICATIONS.
- IN AREAS OF CONCRETE SIDEWALK AND PAVEMENT REMOVAL, COMPLETELY REMOVE EXISTING PAVEMENT (OR SIDEWALK) TO THE DEFINED LIMITS OF CLASS 1 EXCAVATION. BACKFILL WITH FURNISHED SUBSOIL UP TO 4" BELOW FINAL GRADE, FOLLOWED BY 4" TOPSOIL AND TURFGRASS SOD ESTABLISHMENT.
- IN AREAS OF NEWLY PROPOSED CONCRETE SIDEWALK, TYPE 2 PAVEMENT OR ASPHALT SHARED USE PATH, COMPLETELY REMOVE EXISTING PAVEMENT (OR SIDEWALK) TO THE DEFINED LIMITS OF CLASS 1 EXCAVATION. BACKFILL WITH COMMON BORROW TO THE BOTTOM OF SIDEWALK, TYPE 2 PAVEMENT OR SHARED USE PATH.



60% PLANS
NOVEMBER 2021

CITY OF TAKOMA PARK
NEW AVE BIKEWAY, SECTION B
MD 650 (NEW HAMPSHIRE AVENUE)
POPLAR AVE TO AUBURN AVE
SHA TRACKING NO. 20-AP-MO-020-XX

ROADWAY PLAN

SCALE: 1"=20'	DATE: NOVEMBER 2021	CONTRACT NO.: T.B.D.
DESIGNED BY: SAB	COUNTY: MONTGOMERY	
DRAWN BY: SAB	LOGMILE:	
CHECKED BY: RJG		
F.A.P. NO.: T.B.D.		
DRAWING NO.: PS-01	1 OF 4	SHEET NO.: 18 OF 44



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PLOTTED: 11/4/2021
FILE: \\ad.rkk.com\its\Cloud\Projects\2020\20007_NewAveSecB\CADD\Plans\pHD-0001_NewAveBike_B.dgn

5 INCH CONCRETE SIDEWALK			
STA. 105 + 50.00 TO STA. 105 + 58.02, LT		94	SF
STA. 109 + 21.39 TO STA. 109 + 46.54, LT		378	SF
STA. 109 + 77.71 TO STA. 110 + 15.72, LT		184	SF
8 INCH PLAIN PORTLAND CEMENT CONCRETE MIX 9			
STA. 110 + 06.93 TO STA. 111 + 50.00, LT		51	SY
ASPHALT SHARED USE PATH			
STA. 105 + 58.02 TO STA. 109 + 21.39, LT		448	SY

DETECTABLE WARNING SURFACE FOR CURB RAMPS			
STA. 109 + 37.12 TO STA. 109 + 46.54, LT		26	SF
STA. 109 + 44.69 TO STA. 109 + 46.44, LT		10	SF
STA. 109 + 77.71 TO STA. 109 + 83.87, LT		12	SF
2' MODIFIED MONOLITHIC CONCRETE MEDIAN TYPE A-1 (MD STD. 645.01)			
STA. 109 + 84.25 TO STA. 111 + 50.00, LT		166	LF
MDOT SHA STANDARD TYPE A CURB ANY HEIGHT OR DEPTH			
STA. 106 + 63.78 TO STA. 107 + 12.74, LT		199	LF
STA. 109 + 21.36 TO STA. 109 + 47.11, LT		33	LF
STA. 109 + 77.45 TO STA. 110 + 15.73, LT		39	LF
STA. 109 + 80.91 TO STA. 109 + 93.90, LT		13	LF

MDOT SHA STANDARD TYPE A COMBINATION CURB AND GUTTER ANY HEIGHT OR DEPTH			
STA. 105 + 50.00 TO STA. 105 + 66.47, LT		17	LF
STA. 109 + 07.92 TO STA. 109 + 48.48, LT		84	LF
STA. 109 + 72.90 TO STA. 110 + 31.69, LT		80	LF
UNIT PAVERS - TYPE 2			
STA. 107 + 78.96 TO STA. 109 + 08.14, LT		258	SF

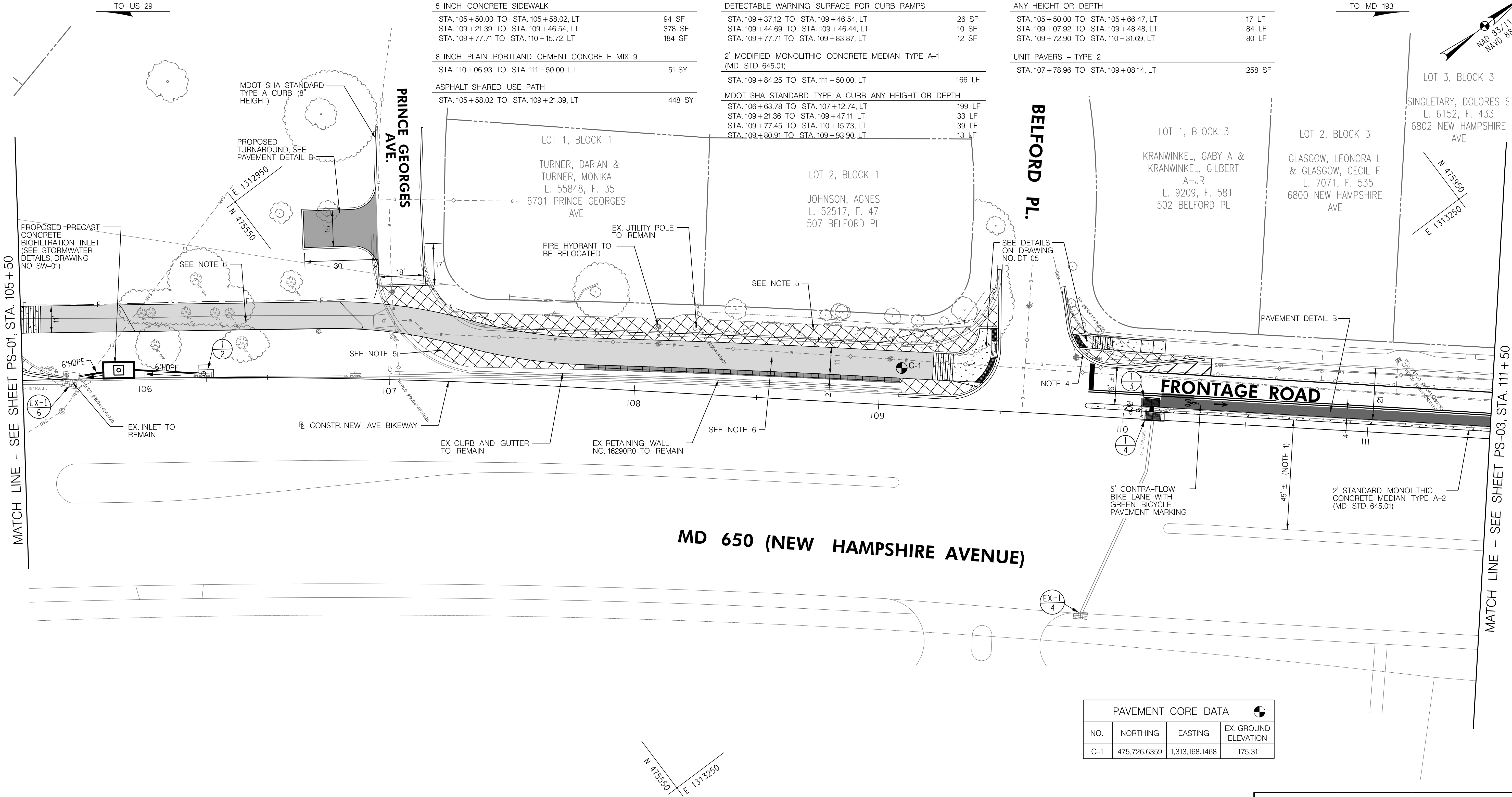
TO US 29

TO MD 193

LOT 3, BLOCK 3

SINGLETARY, DOLORES S
L. 6152, F. 433
6802 NEW HAMPSHIRE AVE

N 47590
E 1313250



MD 650 (NEW HAMPSHIRE AVENUE)

PAVEMENT CORE DATA			
NO.	NORTHING	EASTING	EX. GROUND ELEVATION
C-1	475,726.6359	1,313,168.1468	175.31

NOTES

- A GEOMETRIC SHIFT ALONG MD 650 SOUTHBOUND IS REQUIRED BETWEEN BELFORD PLACE AND AUBURN AVENUE TO ACCOMMODATE THE PROPOSED 5' CONTRA-FLOW BIKE LANE OR 8' SHARED USE PATH.
- ALL CONCRETE CURB AND GUTTER WILL BE PAID FOR AS STANDARD TYPE A COMBINATION CURB & GUTTER, ANY HEIGHT OR DEPTH.
- ALL CONCRETE CURB WILL BE PAID FOR AS STANDARD TYPE A CURB, ANY HEIGHT OR DEPTH (EXCEPTION BEING TYPE B CURB FOR MD 650 MEDIAN WORK).
- PARKING TO BE REMOVED ALONG FRONTAGE ROAD BETWEEN STA. 109 + 75 LT. AND STA. 110 + 30 LT.
- IN AREAS OF CONCRETE SIDEWALK AND PAVEMENT REMOVAL, COMPLETELY REMOVE EXISTING PAVEMENT (OR SIDEWALK) TO THE DEFINED LIMITS OF CLASS 1 EXCAVATION. BACKFILL WITH FURNISHED SUBSOIL UP TO 4" BELOW FINAL GRADE FOLLOWED BY 4" TOPSOIL AND TURFGRASS SOD ESTABLISHMENT.
- IN AREAS OF NEWLY PROPOSED CONCRETE SIDEWALK, TYPE 2 PAVER OR ASPHALT SHARED USE PATH, COMPLETELY REMOVE EXISTING PAVEMENT (OR SIDEWALK) TO THE DEFINED LIMITS OF CLASS 1 EXCAVATION. BACKFILL WITH COMMON BORROW TO THE BOTTOM OF SIDEWALK, TYPE 2 PAVER OR SHARED USE PATH.

LEGEND

	5 INCH CONCRETE SIDEWALK
	CONCRETE DRIVEWAY
	SPECIALTY PAVER - TYPE 2
	ASPHALT SHARED USE PATH
	FULL DEPTH PAVING
	SIDEWALK AND PAVEMENT REMOVAL (PAID FOR AS CLASS 1 EXCAVATION)



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CITY OF TAKOMA PARK
NEW AVE BIKEWAY, SECTION B
MD 650 (NEW HAMPSHIRE AVENUE)
POPLAR AVE TO AUBURN AVE
SHA TRACKING NO. 20-AP-MO-020-XX

ROADWAY PLAN

SCALE 1"=20' DATE NOVEMBER 2021 CONTRACT NO. T.B.D.

DESIGNED BY SAB COUNTY MONTGOMERY
DRAWN BY SAB LOGMILE
CHECKED BY RJG
F.A.P. NO. T.B.D.

DRAWING NO. PS-02 2 OF 4 SHEET NO. 19 OF 44

60% PLANS
NOVEMBER 2021

PLOTTED: 11/4/2021
FILE: \\ad.rkk.com\its\Cloud\Projects\2020\20007_NewAveSecB\CADD\Plans\pHD-0002_NewAveBike_B.dgn

BY: sbarefoot -



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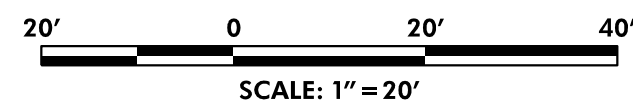
LEGEND

	5 INCH CONCRETE SIDEWALK
	CONCRETE DRIVEWAY
	SPECIALTY PAVER - TYPE 2
	ASPHALT SHARED USE PATH
	FULL DEPTH PAVING
	SIDEWALK AND PAVEMENT REMOVAL (PAID FOR AS CLASS 1 EXCAVATION)

NOTES

- INSTALL 6' LONG (MAXIMUM) WHEEL STOP. MAINTAIN 2' CLEARANCE FROM THE PROPOSED CURB LINE AND 3' MINIMUM CLEARANCE BETWEEN ADJACENT WHEEL STOPS.
- A GEOMETRIC SHIFT ALONG MD 650 SOUTHBOUND IS REQUIRED BETWEEN BELFORD PLACE AND AUBURN AVENUE TO ACCOMMODATE THE PROPOSED 5' CONTRA-FLOW BIKE LANE OR 8' SHARED USE PATH.
- COORDINATE REMOVAL OF THE EXISTING LIGHT POLE WITHIN THE PUBLIC RIGHT-OF-WAY WITH THE NEW HAMPSHIRE AVENUE SHOPPING CENTER.
- ALL CONCRETE CURB AND GUTTER WILL BE PAID FOR AS STANDARD TYPE A COMBINATION CURB & GUTTER, ANY HEIGHT OR DEPTH.
- ALL CONCRETE CURB WILL BE PAID FOR AS STANDARD TYPE A CURB, ANY HEIGHT OR DEPTH (EXCEPTION BEING TYPE B CURB FOR MD 650 MEDIAN WORK).
- SEE TRAFFIC SIGNAL PLANS FOR IMPACTS TO EXISTING TRAFFIC SIGNAL EQUIPMENT AND PROPOSED MODIFICATIONS.
- PARKING TO BE REMOVED ALONG FRONTAGE ROAD BETWEEN STA. 113+40 AND STA. 115+20 LT.
- RIGHT OF WAY REQUIRED FOR BIKEWAY / PEDESTRIAN RAMP CONSTRUCTION FROM NEW HAMPSHIRE AVENUE SHOPPING CENTER LLC BETWEEN STA. 115+50 AND 115+75 LT.
- IN AREAS OF CONCRETE, SIDEWALK AND PAVEMENT REMOVAL, COMPLETELY REMOVE EXISTING PAVEMENT (OR SIDEWALK) TO THE DEFINED LIMITS OF CLASS 1 EXCAVATION. BACKFILL WITH FURNISHED SUBSOIL UP TO 4' BELOW FINAL GRADE, FOLLOWED BY 4" TOPSOIL AND TURFGRASS SOD ESTABLISHMENT.
- IN AREAS OF NEWLY PROPOSED CONCRETE SIDEWALK, TYPE 2 PAVER OR ASPHALT SHARED USE PATH, COMPLETELY REMOVE EXISTING PAVEMENT (OR SIDEWALK) TO THE DEFINED LIMITS OF CLASS 1 EXCAVATION. BACKFILL WITH COMMON BORROW TO THE BOTTOM OF SIDEWALK, TYPE 2 PAVER OR SHARED USE PATH.

PAVEMENT CORE DATA			
NO.	NORTHING	EASTING	EX. GROUND ELEVATION
C-2	476,137.3884	1,313,520.9515	208.00



60% PLANS

NOVEMBER 2021

PLOTTED: 11/4/2021
FILE: \\ad.rkk.com\its\Cloud\Projects\2020\20007_NewAveSecB\CADD\Plans\pHD-0003_NewAveBike_B.dgn

CITY OF TAKOMA PARK
NEW AVE BIKEWAY, SECTION B
MD 650 (NEW HAMPSHIRE AVENUE)
POPLAR AVE TO AUBURN AVE
SHA TRACKING NO. 20-AP-MO-020-XX

ROADWAY PLAN

SCALE 1"=20' DATE NOVEMBER 2021 CONTRACT NO. T.B.D.

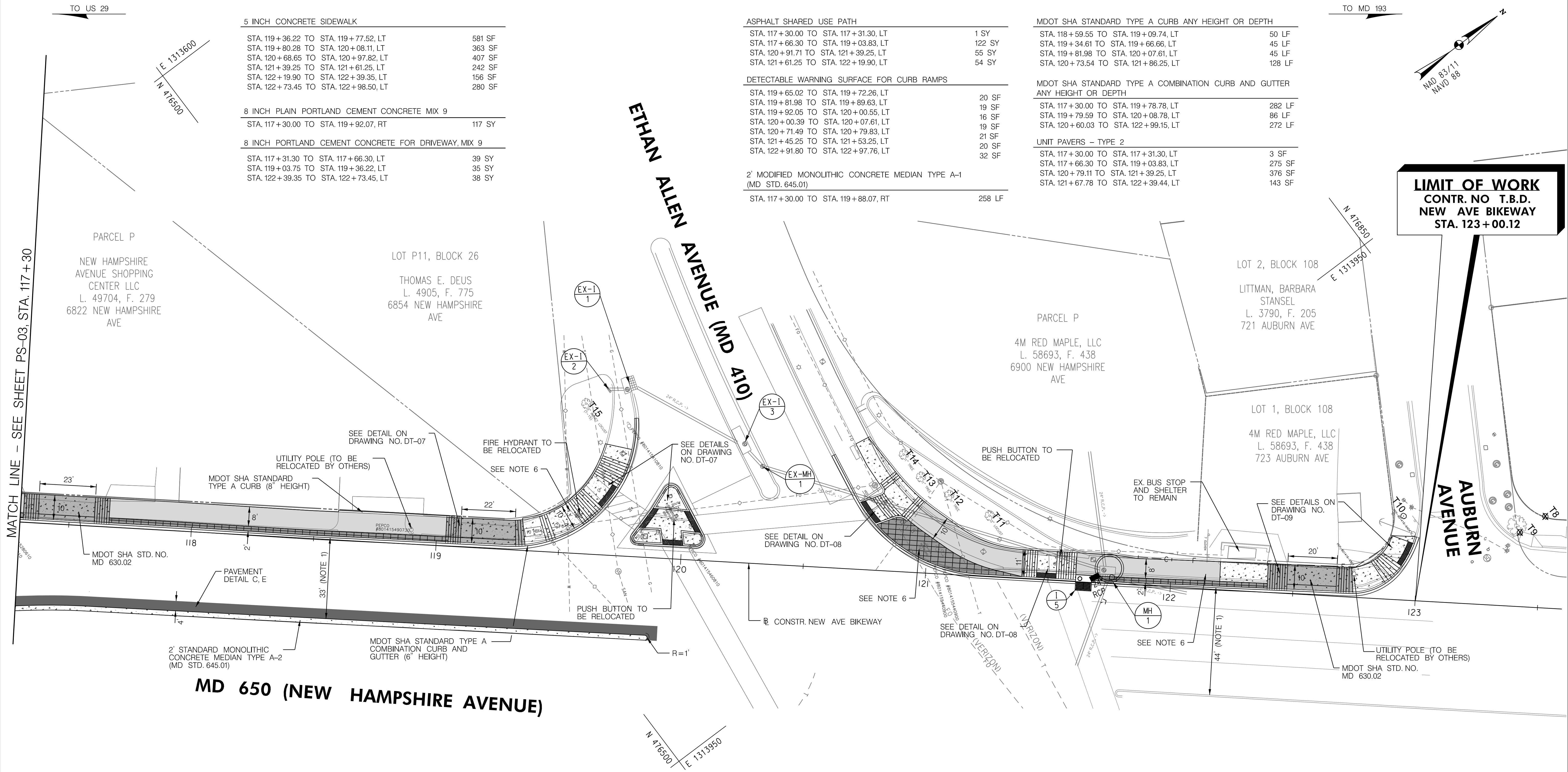
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DRAWN BY SAB LOGMILE
CHECKED BY RJG
F.A.P. NO. T.B.D.

DRAWING NO. PS03 3 OF 4 SHEET NO. 20 OF 44

MATCH LINE - SEE SHEET PS-02, STA. 111+50

MATCH LINE - SEE SHEET PS-04, STA. 117+30

PS-03



MATCH LINE - SEE SHEET PS-03, STA. 117+30

TO US 29

TO MD 193

PS-04

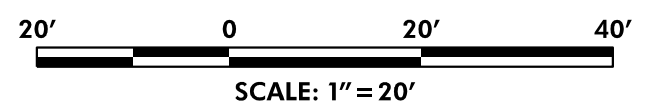
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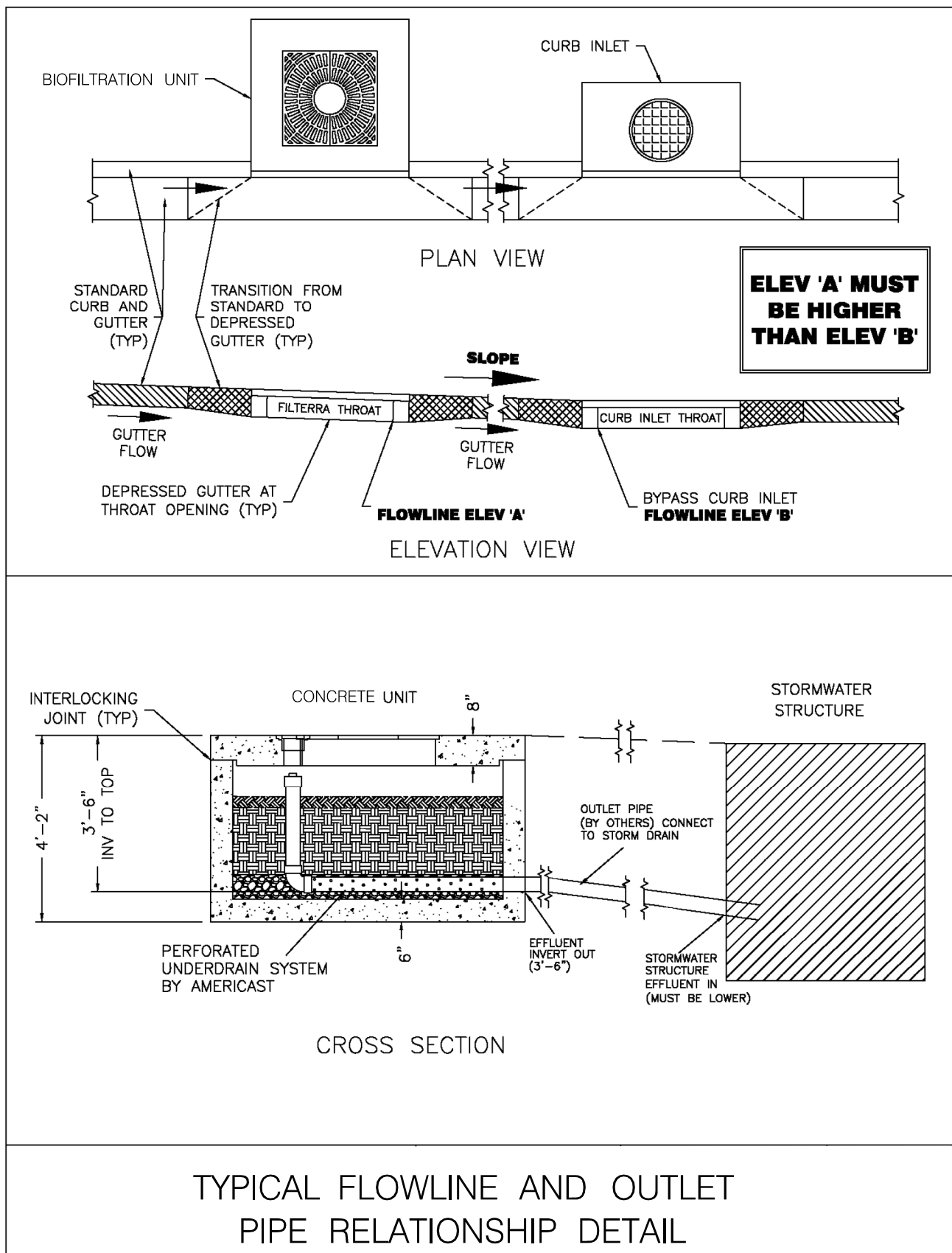
	5 INCH CONCRETE SIDEWALK
	CONCRETE DRIVEWAY
	SPECIALTY PAVER - TYPE 2
	ASPHALT SHARED USE PATH
	FULL DEPTH PAVING
	SIDEWALK AND PAVEMENT REMOVAL (PAID FOR AS CLASS 1 EXCAVATION)

- NOTES**
- A GEOMETRIC SHIFT ALONG MD 650 SOUTHBOUND IS REQUIRED BETWEEN BELFORD PLACE AND AUBURN AVENUE TO ACCOMMODATE THE PROPOSED 5' CONTRA-FLOW BIKE LANE OR 8' SHARED USE PATH.
 - ALL CONCRETE CURB AND GUTTER WILL BE PAID FOR AS STANDARD TYPE A COMBINATION CURB & GUTTER, ANY HEIGHT OR DEPTH.
 - ALL CONCRETE CURB WILL BE PAID FOR AS STANDARD TYPE A CURB, ANY HEIGHT OR DEPTH (EXCEPTION BEING TYPE B CURB FOR MD 650 MEDIAN WORK).
 - SEE TRAFFIC SIGNAL PLANS FOR IMPACTS TO EXISTING TRAFFIC SIGNAL EQUIPMENT AND PROPOSED MODIFICATIONS.
 - IN AREAS OF CONCRETE SIDEWALK AND PAVEMENT REMOVAL, COMPLETELY REMOVE EXISTING PAVEMENT (OR SIDEWALK) TO THE DEFINED LIMITS OF CLASS 1 EXCAVATION. BACKFILL WITH FURNISHED SUBSOIL UP TO 4" BELOW FINAL GRADE, FOLLOWED BY 4" TOPSOIL AND TURFGRASS SOD ESTABLISHMENT.
 - IN AREAS OF NEWLY PROPOSED CONCRETE SIDEWALK, TYPE 2 PAVER OR ASPHALT SHARED USE PATH, COMPLETELY REMOVE EXISTING PAVEMENT (OR SIDEWALK) TO THE DEFINED LIMITS OF CLASS 1 EXCAVATION. BACKFILL WITH COMMON BORROW TO THE BOTTOM OF SIDEWALK, TYPE 2 PAVER OR SHARED USE PATH.



60% PLANS NOVEMBER 2021	ROADWAY PLAN		
	SCALE <u>1"=20'</u> DATE <u>NOVEMBER 2021</u> CONTRACT NO. <u>T.B.D.</u>		
	DESIGNED BY <u>SAB</u>	COUNTY <u>MONTGOMERY</u>	
	DRAWN BY <u>SAB</u>	LOGMILE <u></u>	
CHECKED BY <u>RJG</u>		F.A.P. NO. <u>T.B.D.</u>	
DRAWING NO. <u>PS04</u>		4 OF 4	SHEET NO. 21 OF 44

PLOTTED: 11/4/2021
FILE: \\ad.rkk.com\its\Cloud\Projects\2020\20007_NewAveSecB\CADD\Plans\pHD-0004_NewAveBike_B.dgn



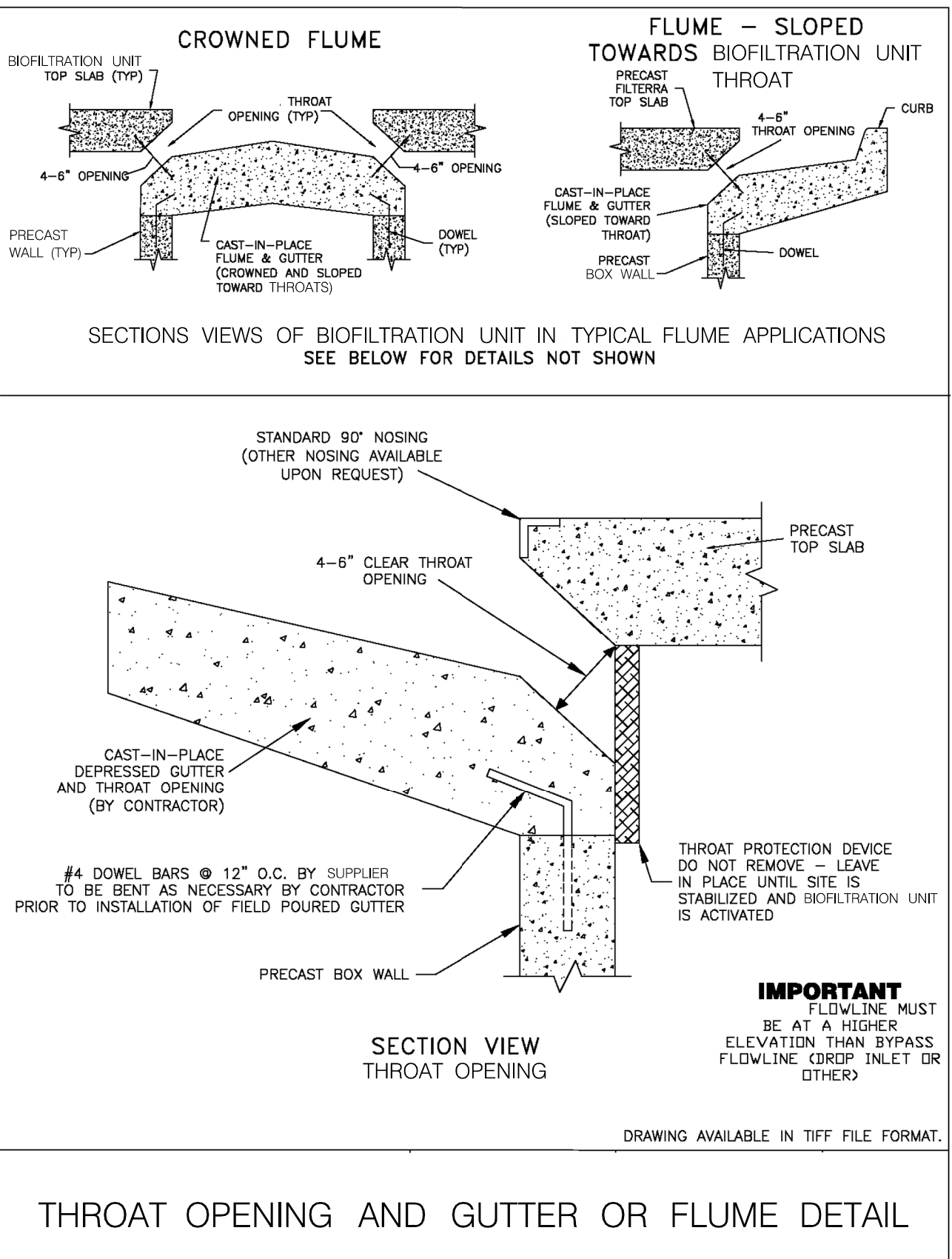
Standard Plan Notes

Construction & Installation

- Each unit shall be constructed at the locations and elevations according to the sizes shown on the approved drawings. Any modifications to the elevation or location shall be at the direction of and approved by the Engineer.
- If the Pre-cast Concrete Biofiltration Unit is stored before installation, the top slab must be placed on the box using 2x4 wood provided, to prevent any contamination from the site. All internal fittings supplied (if any), must be left in place as per the delivery.
- The unit shall be placed on a compacted sub-grade with a minimum 6-inch gravel base matching the final grade of the curb line in the area of the unit. The unit to be placed such that the unit and top slab match the grade of the curb in the area of the unit. Compact undisturbed sub-grade materials to 95% of maximum density at +1- 2% of optimum moisture. Unsuitable material below sub-grade shall be replaced to the site engineer's approval.
- Outlet connections shall be aligned and sealed to meet the approved drawings with modifications necessary to meet site conditions and local regulations.
- Once the unit is set, the internal wooden forms and protective mesh cover must be left intact. Remove only the temporary wooden shipping blocks between the box and top slab. The top lid should be sealed onto the box section before backfilling, using a non-shrink grout, butyl rubber or similar waterproof seal. The boards on top of the lid and boards sealed in the unit's throat must **NOT** be removed. The Supplier/Manufacturer will remove these sections at the time of activation. Backfilling should be performed in a careful manner, bringing the appropriate fill material up in 6" lifts on all sides. Precast sections shall be set in a manner that will result in a watertight joint. In all instances, installation of Pre-cast Concrete Biofiltration Unit shall conform to ASTM specification C891 "Standard Practice for Installation of Underground Precast Utility Structures", unless directed otherwise in contract documents.
- Curb and gutter construction (where present) shall ensure that the flow-line of the Pre-cast Concrete Biofiltration Units is at a greater elevation than the flow-line of the bypass structure or relief (drop inlet, curb cut or similar). Failure to comply with this guideline may cause failure and/or damage to the Pre-cast Concrete Biofiltration Unit environmental device.
- Each Pre-cast Concrete Biofiltration Unit must receive adequate irrigation to ensure survival of the living system during periods of drier weather. This may be achieved through gutter flow or through the tree grate.

Activation

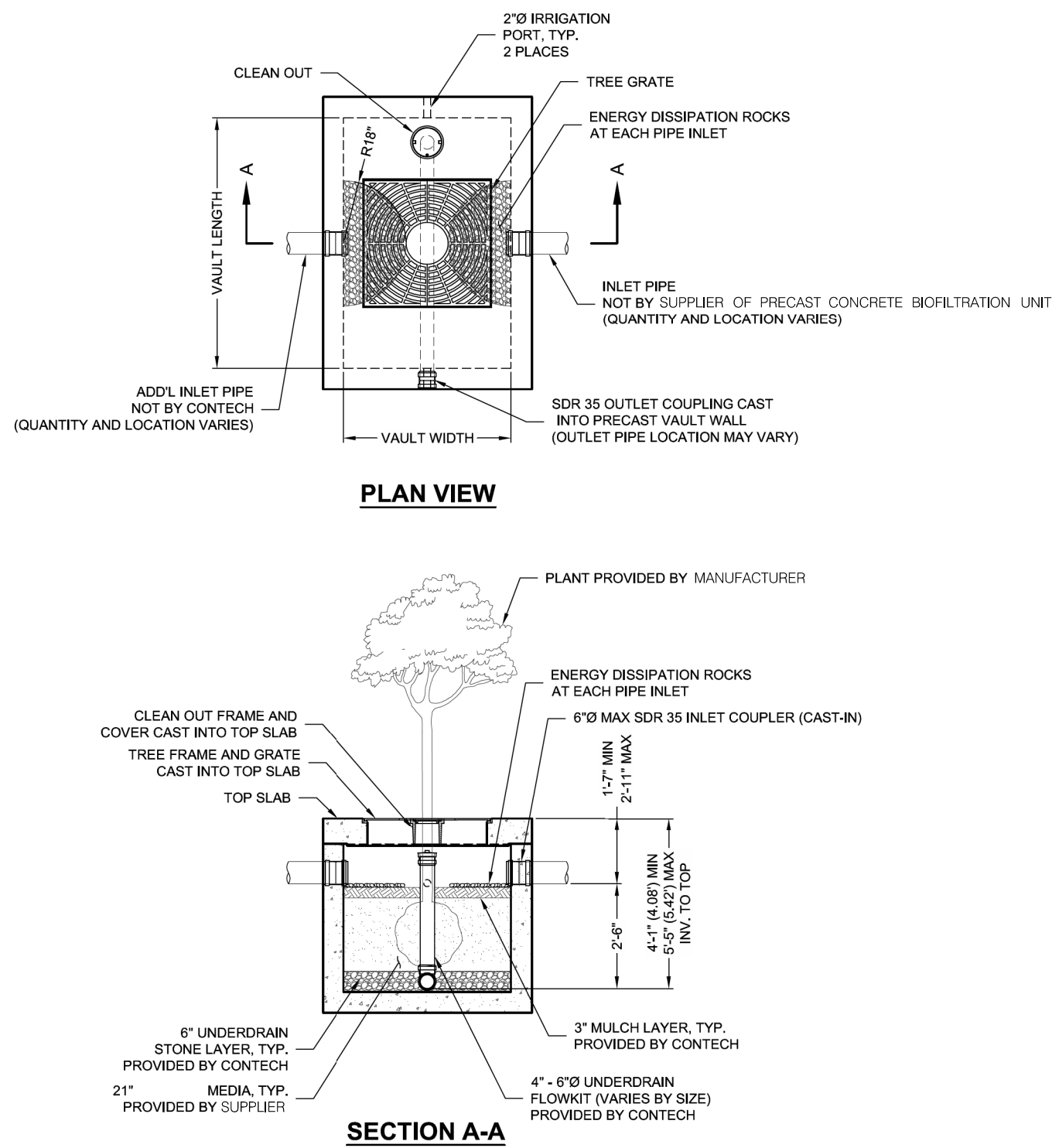
- Activation of the Pre-cast Concrete Biofiltration Unit is performed **ONLY** by the Supplier. Purchaser is responsible for Pre-cast Concrete Biofiltration Unit inlet protection and subsequent clean out cost. This process cannot commence until the project site is fully stabilized and cleaned (full landscaping, grass cover, final paving and street sweeping completed), negating the chance of construction materials contaminating the Pre-cast Concrete Biofiltration Unit system. Care shall be taken during construction not to damage the protective throat and top plates.
- Activation includes installation of plant(s) and mulch layers as necessary.



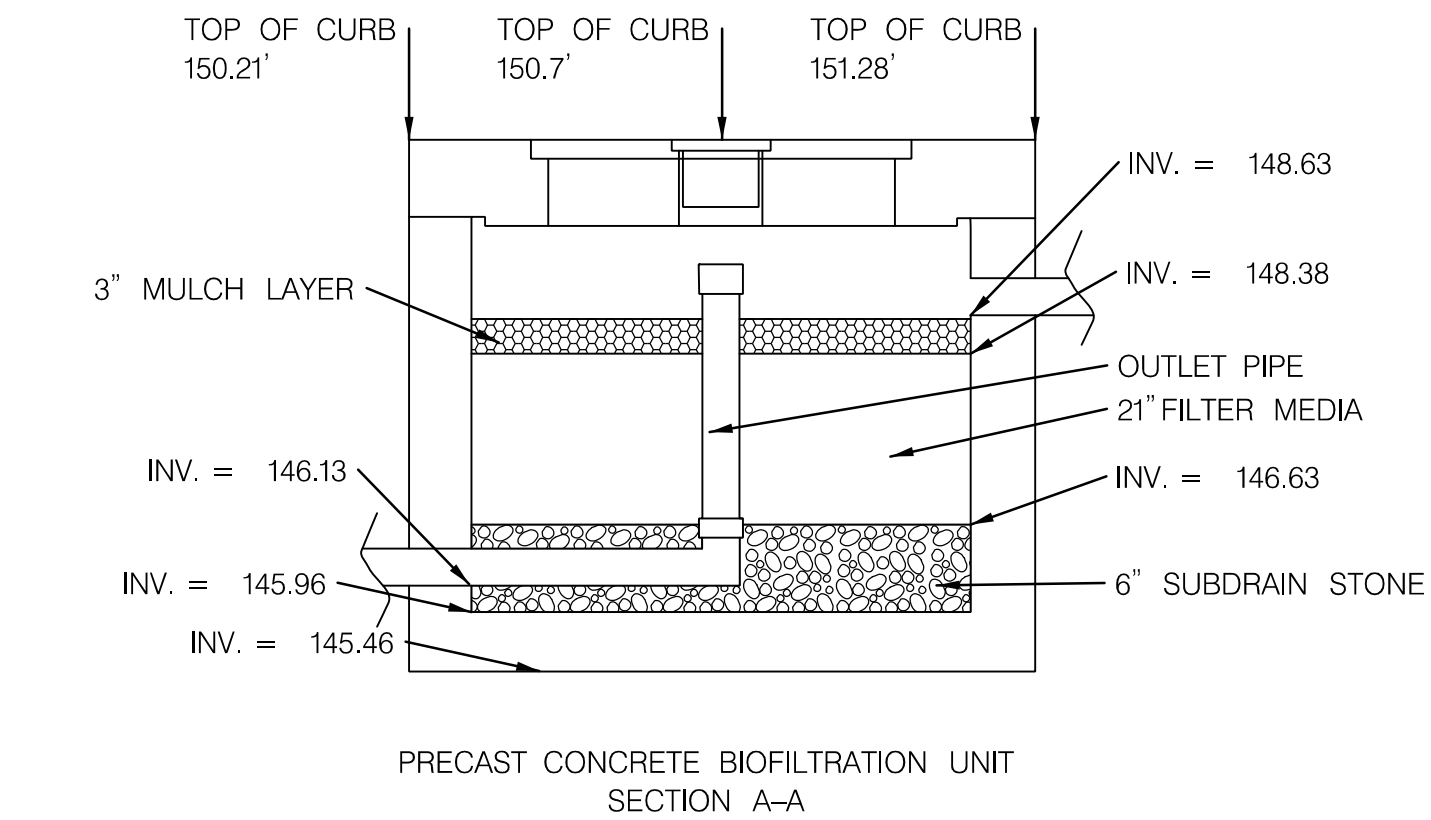
THROAT OPENING AND GUTTER OR FLUME DETAIL

Maintenance

- Each correctly installed Pre-cast Concrete Biofiltration Unit is to be maintained by the Supplier, or a Supplier approved contractor for a minimum period of 1 year. The cost of this service is to be included in the price of each Pre-cast Concrete Biofiltration Unit. Extended maintenance contracts are available at extra cost upon request.
- Annual maintenance consists of a maximum of (2) scheduled visits. The visits are scheduled seasonally; the spring visit aims to clean up after winter loads including salts and sands. The fall visit helps the system by removing excessive leaf litter.
- Each maintenance visit consists of the following tasks:
 - Pre-cast Concrete Biofiltration Unit inspection
 - Foreign debris, silt, mulch & trash removal
 - Filter media evaluation and recharge as necessary
 - Plant health evaluation and pruning or replacement as necessary
 - Replacement of mulch
 - Disposal of all maintenance refuse items
 - Maintenance records updated and stored (reports available upon request)
- The beginning and ending date of Supplier's obligation to maintain the installed system shall be determined by the Supplier at the time the system is activated. Owners must promptly notify the Supplier of any damage to the plant(s), which constitute(s) an integral part of the bioretention technology.

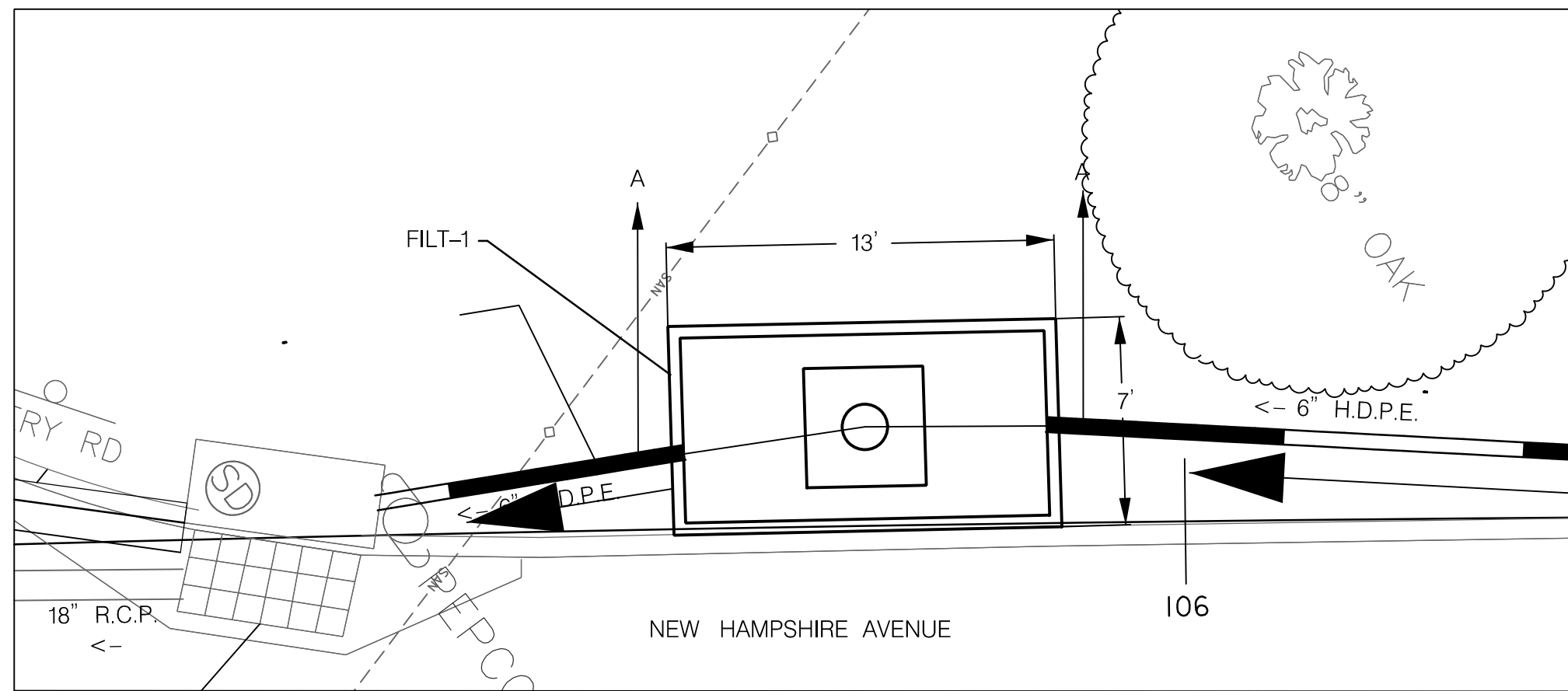


OFFLINE PIPE CONFIGURATION DETAIL



NOTE 1: BOTTOM OF PRECAST CONCRETE BIOFILTRATION UNIT INTERNAL MEDIA TO BE AT FLAT, CONSISTENT GRADE. TOP SLAB TO BE SLOPED IN ORDER TO MATCH ROADWAY GRADE. DETAILS SHOWN COMBINE 7' x13' MODIFIED PRECAST UNIT WITH STANDARD DETAIL FOR OFFLINE INFLOW. STRUCTURE SHALL BE PLACED ACCORDING TO ELEVATIONS AS SHOWN ON PLANS. DETAILS SHOWN ARE PRELIMINARY AS COORDINATION WITH SUPPLIER IS FORTHCOMING. SEE DRAINAGE PROFILES FOR FURTHER DETAILS.

NOTE 2: SEE SPECIAL PROVISION SP-SECTION 300 - PRECAST CONCRETE BIOFILTRATION UNIT FOR MORE INFORMATION. THE CONTRACTOR SHALL BE RESPONSIBLE TO ENSURE THAT THE UNIT BE INSTALLED WILL MEET ALL THE SWM REGULATIONS AND PERMIT REQUIREMENTS FOR WATER QUALITY CONTROL AS SET FORTH BY THE CITY OF TAKOMA PARK.



FILT-1, PLAN VIEW

CITY OF TAKOMA PARK
NEW AVE BIKEWAY, SECTION B
MD 650 (NEW HAMPSHIRE AVENUE)
POPLAR AVE TO AUBURN AVE
SHA TRACKING NO. 20-AP-MO-020-XX

STORMWATER DETAILS

SCALE NOT TO SCALE	DATE NOVEMBER 2021	CONTRACT NO. T.B.D.
DESIGNED BY AGB	COUNTY MONTGOMERY	
DRAWN BY AGB	LOGMILE	
CHECKED BY SBP		
F.A.P. NO. T.B.D.		
DRAWING NO. SW-01	OF 01	SHEET NO. 22 OF 44

PLOTTED: 11/4/2021
FILE: \\ad.rkk.com\its\Cloud\Projects\2020\20007_NewAveSecB\CADD\Plans\pSW-D001_NewAveBike_B.dgn

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STANDARD EROSION AND SEDIMENT CONTROL NOTES

1. THE PERMITTEE SHALL NOTIFY THE DEPARTMENT OF PERMITTING SERVICES (DPS) FORTY EIGHT (48) HOURS BEFORE COMMENCING ANY LAND DISTURBING ACTIVITY AND, UNLESS WAIVED BY THE DEPARTMENT, SHALL BE REQUIRED TO HOLD A PRE-CONSTRUCTION MEETING BETWEEN THEM OR THEIR REPRESENTATIVE, THEIR ENGINEER AND AN AUTHORIZED REPRESENTATIVE OF THE DEPARTMENT.

2. THE PERMITTEE MUST OBTAIN INSPECTION AND APPROVAL BY DPS AT THE FOLLOWING POINTS:
2.1. AT THE REQUIRED PRE-CONSTRUCTION MEETING.
2.2. FOLLOWING INSTALLATION OF SEDIMENT CONTROL MEASURES AND PRIOR TO ANY OTHER LAND DISTURBING ACTIVITY.
2.3. DURING THE INSTALLATION OF A SEDIMENT BASIN OR STORMWATER MANAGEMENT STRUCTURE AT THE REQUIRED INSPECTION POINTS (SEE INSPECTION CHECKLIST ON PLAN). NOTIFICATION PRIOR TO COMMENCING CONSTRUCTION IS MANDATORY.
2.4. PRIOR TO REMOVAL OR MODIFICATION OF ANY SEDIMENT CONTROL STRUCTURE(S).
2.5. PRIOR TO FINAL ACCEPTANCE.

3. THE PERMITTEE SHALL CONSTRUCT ALL EROSION AND SEDIMENT CONTROL MEASURES PER THE APPROVED PLAN AND CONSTRUCTION SEQUENCE, SHALL HAVE THEM INSPECTED AND APPROVED BY THE DEPARTMENT PRIOR TO BEGINNING ANY OTHER LAND DISTURBANCES, SHALL ENSURE THAT ALL RUNOFF FROM DISTURBED AREAS IS DIRECTED TO THE SEDIMENT CONTROL DEVICES, AND SHALL NOT REMOVE ANY EROSION OR SEDIMENT CONTROL MEASURE WITHOUT PRIOR PERMISSION FROM THE DEPARTMENT.

4. THE PERMITTEE SHALL PROTECT ALL POINTS OF CONSTRUCTION INGRESS AND EGRESS TO PREVENT THE DEPOSITION OF MATERIALS ONTO TRAVERSED PUBLIC THOROUGHFARE(S). ALL MATERIALS DEPOSITED ONTO PUBLIC THOROUGHFARE(S) SHALL BE REMOVED IMMEDIATELY.

5. THE PERMITTEE SHALL INSPECT PERIODICALLY AND MAINTAIN CONTINUOUSLY IN EFFECTIVE OPERATING CONDITION, ALL EROSION AND SEDIMENT CONTROL MEASURES UNTIL SUCH TIME AS THEY ARE REMOVED WITH PRIOR PERMISSION FROM THE DEPARTMENT. THE PERMITTEE IS RESPONSIBLE FOR IMMEDIATELY REPAIRING OR REPLACING ANY SEDIMENT CONTROL MEASURES WHICH HAVE BEEN DAMAGED OR REMOVED BY THE PERMITTEE OR ANY OTHER PERSON.

6. FOLLOWING INITIAL SOIL DISTURBANCE OR RE-DISTURBANCE, PERMANENT OR TEMPORARY STABILIZATION MUST BE COMPLETED WITHIN:
6.1. THREE (3) CALENDAR DAYS AS TO THE SURFACE OF ALL PERIMETER DIKES, SWALES, DITCHES, PERIMETER SLOPES AND ALL SLOPES STEEPER THAN 3 HORIZONTAL TO 1 VERTICAL (3:1); AND
6.2. SEVEN (7) CALENDAR DAYS AS TO ALL OTHER DISTURBED OR GRADED AREAS ON THE PROJECT SITE NOT UNDER ACTIVE GRADING.

ALL OTHER DISTURBED AREA OUTSIDE OF THE PERIMETER SEDIMENT CONTROL SYSTEM MUST BE MINIMIZED AND STABILIZED IMMEDIATELY. MAINTENANCE MUST BE PERFORMED AS NECESSARY TO ENSURE CONTINUED STABILIZATION.

7. THE PERMITTEE SHALL APPLY SOD, SEED, AND ANCHORED STRAW MULCH, OR OTHER APPROVED STABILIZATION MEASURES TO ALL DISTURBED AREAS WITHIN SEVEN (7) CALENDAR DAYS AFTER STRIPPING AND GRADING ACTIVITIES HAVE CEASED ON THAT AREA. MAINTENANCE SHALL BE PERFORMED AS NECESSARY TO ENSURE CONTINUED STABILIZATION. ACTIVE CONSTRUCTION AREAS SUCH AS BORROW OR STOCKPILE AREAS, ROADWAY IMPROVEMENTS, AND AREAS WITHIN FIFTY (50) FEET OF A BUILDING UNDER CONSTRUCTION MAY BE EXEMPT FROM THIS REQUIREMENT, PROVIDED THAT EROSION AND SEDIMENT CONTROL MEASURES ARE INSTALLED AND MAINTAINED TO PROTECT THOSE AREAS.

8. PRIOR TO REMOVAL OF SEDIMENT CONTROL MEASURES, THE PERMITEE SHALL STABILIZE ALL CONTRIBUTORY DISTURBED AREAS WITH REQUIRED SOIL AMENDMENTS AND TOPSOIL, USING SOD OR AN APPROVED PERMANENT SEED MIXTURE AND AN APPROVED ANCHORED MULCH. WOOD FIBER MULCH MAY ONLY BE USED IN SEEDING SEASON WHEN THE SLOPE DOES NOT EXCEED 10% AND GRADING HAS BEEN DONE TO PROMOTE SHEET FLOW DRAINAGE. AREAS BROUGHT TO FINISHED GRADE DURING THE SEEDING SEASON SHALL BE PERMANENTLY STABILIZED WITHIN SEVEN (7) CALENDAR DAYS OF ESTABLISHMENT. WHEN PROPERTY IS BROUGHT TO FINISHED GRADE DURING THE MONTHS OF NOVEMBER THROUGH FEBRUARY, AND PERMANENT STABILIZATION IS FOUND TO BE IMPRACTICAL, AN APPROVED TEMPORARY SEED AND STRAW ANCHORED MULCH SHALL BE APPLIED TO DISTURBED AREAS. THE FINAL PERMANENT STABILIZATION OF SUCH PROPERTY SHALL BE COMPLETED PRIOR TO THE FOLLOWING APRIL 15.

9. THE SITE PERMIT, WORK, MATERIALS, APPROVED SC/SM PLANS, AND TEST REPORTS SHALL BE AVAILABLE AT THE SITE FOR INSPECTION BY DULY AUTHORIZED OFFICIALS OF MONTGOMERY COUNTY.

10. SURFACE DRAINAGE FLOWS OVER UNSTABILIZED CUT AND FILL SLOPES SHALL BE CONTROLLED BY EITHER PREVENTING DRAINAGE FLOWS FROM TRAVERSING THE SLOPES OR BY INSTALLING MECHANICAL DEVICES TO LOWER THE WATER DOWN SLOPE WITHOUT CAUSING EROSION. DIKES SHALL BE INSTALLED AND MAINTAINED AT THE TOP OF CUT OR FILL SLOPES UNTIL THE SLOPE AND DRAINAGE AREA TO IT ARE FULLY STABILIZED, AT WHICH TIME THEY MUST BE REMOVED AND FINAL GRADING DONE TO PROMOTE SHEET FLOW DRAINAGE. MECHANICAL DEVICES MUST BE PROVIDED AT POINTS OF CONCENTRATED FLOW WHERE EROSION IS LIKELY TO OCCUR.

11. PERMANENT SWALES OR OTHER POINTS OF CONCENTRATED WATER FLOW SHALL BE STABILIZED WITHIN THREE (3) CALENDAR DAYS OF ESTABLISHMENT WITH SOD OR SEED WITH AN APPROVED EROSION CONTROL MATTING OR BY OTHER APPROVED STABILIZATION MEASURES.

12. SEDIMENT CONTROL DEVICES SHALL BE REMOVED, WITH PERMISSION OF THE DEPARTMENT, WITHIN THIRTY (30) CALENDAR DAYS FOLLOWING ESTABLISHMENT OF PERMANENT STABILIZATION IN ALL CONTRIBUTORY DRAINAGE AREAS. STORMWATER MANAGEMENT STRUCTURES USED TEMPORARILY FOR SEDIMENT CONTROL SHALL BE CONVERTED TO THE PERMANENT CONFIGURATION WITHIN THIS TIME PERIOD AS WELL.

13. NO PERMANENT CUT OF FILL SLOPE WITH A GRADIENT STEEPER THAN 3:1 WILL BE PERMITTED IN LAWN MAINTENANCE AREAS OR ON RESIDENTIAL LOTS. A SLOPE GRADIENT OF 2:1 WILL BE PERMITTED IN NON-MAINTENANCE AREAS PROVIDED THAT THOSE AREAS ARE INDICATED ON THE EROSION AND SEDIMENT CONTROL PLAN WITH A LOW-MAINTENANCE GROUND COVER SPECIFIED FOR PERMANENT STABILIZATION. SLOPE GRADIENT STEEPER THAN 2:1 WILL NOT BE PERMITTED WITH VEGETATIVE STABILIZATION.

14. THE PERMITTEE SHALL INSTALL A SPLASHBLOCK AT THE BOTTOM OF EACH DOWNSPOUT UNLESS THE DOWNSPOUT IS CONNECTED BY A DRAIN LINE TO AN ACCEPTABLE OUTLET.

15. FOR FINISHED GRADING, THE PERMITTEE SHALL PROVIDE ADEQUATE GRADIENTS SO AS TO PREVENT WATER FROM STANDING ON THE SURFACE OF LAWNS MORE THAN TWENTY-FOUR (24) HOURS AFTER THEN END OF A RAINFALL, EXCEPT IN DESIGNATED DRAINAGE COURSES AND SWALE FLOW AREAS, WHICH MAY DRAIN AS LONG AS FORTY-EIGHT (48) HOURS AFTER THE END OF A RAINFALL.

16. SEDIMENT TRAPS OR BASINS ARE NOT PERMITTED WITHIN 20 FEET OF A BUILDING WHICH IS EXISTING OR UNDER CONSTRUCTION. NO BUILDING MAY BE CONSTRUCTED WITHIN 20 FEET OF A SEDIMENT TRAP OR BASIN.
17. ALL INLETS IN NON-SUMP AREAS SHALL HAVE ASPHALT BERMS INSTALLED AT THE TIME OF BASE PAVING ESTABLISHMENT.

18. THE SEDIMENT CONTROL INSPECTOR HAS THE OPTION OF REQUIRING ADDITIONAL SEDIMENT CONTROL MEASURES, AS DEEMED NECESSARY.

19. ALL TRAP ELEVATIONS ARE RELATIVE TO THE OUTLET ELEVATION, WHICH MUST BE ON EXISTING UNDISTURBED GROUND.

20. VEGETATIVE STABILIZATION SHALL BE PERFORMED IN ACCORDANCE WITH THE STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL.

21. SEDIMENT TRAP(S)/BASIN(S) SHALL BE CLEANED OUT AND RESTORED TO THE ORIGINAL DIMENSIONS WHEN SEDIMENT HAS ACCUMULATED TO THE POINT OF ONE-HALF (1/2) THE WET STORAGE DEPTH OF THE TRAP/BASIN (1/4 THE WET STORAGE DEPTH FOR ST-III) OR WHEN REQUIRED BY THE SEDIMENT CONTROL INSPECTOR.

22. SEDIMENT REMOVED FROM TRAPS/BASINS SHALL BE PLACED AND STABILIZED IN APPROVED AREAS, BUT NOT WITHIN A 100-YEAR FLOODPLAIN.

23. ALL SEDIMENT BASINS AND TRAPS MUST BE SURROUNDED WITH A WELDED WIRE SAFETY FENCE. THE FENCE MUST BE AT LEAST 42 INCHES HIGH, HAVE POSTS SPACED NO FARTHER APART THAN 8 FEET, HAVE MESH OPENINGS NO GREATER THAN TWO INCHES IN WIDTH AND FOUR INCHES IN HEIGHT, WITH A MINIMUM OF 14 GAUGE WIRE. SAFETY FENCE MUST BE MAINTAINED IN GOOD CONDITION AT ALL TIMES.

24. NO EXCAVATION IN THE AREAS OF EXISTING UTILITIES IS PERMITTED UNLESS THEIR LOCATION HAS BEEN DETERMINED. CALL "MISS UTILITY" AT 1-800-257-7777, 48 HOURS PRIOR TO THE START OF WORK.

25. OFF SITE SPOIL OR BORROW AREAS MUST HAVE PRIOR APPROVAL BY DPS.

26. SEDIMENT TRAP/BASIN DEWATERING FOR CLEANOUT REPAIR MAY ONLY BE DONE WITH THE DPS INSPECTOR'S PERMISSION. THE INSPECTOR MUST APPROVE THE DEWATERING METHOD FOR EACH APPLICATION. THE FOLLOWING METHODS MAY BE CONSIDERED:
26.1. PUMP DISCHARGE MAY BE DIRECTED TO ANOTHER ON-SITE SEDIMENT TRAP OR BASIN, PROVIDED IT IS OF SUFFICIENT VOLUME AND THE PUMP INTAKE IS FLOATED TO PREVENT AGITATION OR SUCTION OF DEPOSITED SEDIMENTS; OR
26.2. THE PUMP INTAKE MAY UTILIZE A REMOVABLE PUMPING STATION AND MUST DISCHARGE INTO AN UNDISTURBED AREA THROUGH A NON-EROSIVE OUTLET; OR
26.3. THE PUMP INTAKE MAY BE FLOATED AND DISCHARGE INTO A DIRT BAG (12 OZ. NON-WOVEN FABRIC), OR APPROVED EQUIVALENT, LOCATED IN AN UNDISTURBED BUFFER AREA.

REMEMBER: DEWATERING OPERATION AND METHOD MUST HAVE PRIOR APPROVAL BY THE DPS INSPECTOR.

27. THE PERMITTEE MUST NOTIFY THE DEPARTMENT OF ALL UTILITY CONSTRUCTION ACTIVITIES WITHIN THE PERMITTED LIMITS OF DISTURBANCE PRIOR TO THE COMMENCEMENT OF THOSE ACTIVITIES.

28. TOPSOIL MUST BE APPLIED TO ALL PERVIOUS AREA WITHIN THE LIMITS OF DISTURBANCE PRIOR TO PERMANENT STABILIZATION IN ACCORDANCE WITH MDE "STANDARDS AND SPECIFICATIONS FOR SOIL PREPARATION, TOPSOILING, AND SOIL AMENDMENTS."
- LIST OF PREDOMINANT SOIL TYPES
- | SYMBOLS | DESCRIPTION | HSG |
|---------|--|-----|
| G/B | GLENELG-WHEATON-URBAN LAND COMPLEX, 0-8% SLOPES | B |
| G/C | GLENELG-WHEATON-URBAN LAND COMPLEX, 8-15% SLOPES | B |
| SgC | SASSAFRAS-URBAN LAND COMPLEX. 8-15% SLOPES | B |
| Un | URBAN LAND | D |
| WpB | WOODSTOWN-URBAN LAND COMPLEX, 0-8% SLOPES | C |
- OWNER'S/DEVELOPER'S CERTIFICATION
- I/WE HEREBY CERTIFY THAT ALL CLEARING, GRADING, CONSTRUCTION, AND/OR DEVELOPMENT WILL BE DONE PURSUANT TO THIS PLAN AND THAT ANY RESPONSIBLE PERSONNEL INVOLVED IN THE CONSTRUCTION PROJECT WILL HAVE A CERTIFICATE OF ATTENDANCE AT A DEPARTMENT OF NATURAL RESOURCES APPROVED TRAINING PROGRAM FOR THE CONTROL OF SEDIMENT AND EROSION BEFORE BEGINNING THE PROJECT.
- SIGNATURE
MS. ROSALIND GRIGSBY
(301) 891-7205

PLANNER
CITY OF
TAKOMA PARK

PRINTED NAME AND TITLE
- DESIGN CERTIFICATION
- I HEREBY CERTIFY THAT THIS PLAN HAS BEEN PREPARED IN ACCORDANCE WITH THE "2011 MARYLAND STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL," MONTGOMERY COUNTY DEPARTMENT OF PERMITTING SERVICES EXECUTIVE REGULATIONS 5-90, 7-02AM AND 36-90, AND MONTGOMERY COUNTY DEPARTMENT OF PUBLIC WORKS AND TRANSPORTATION "STORM DRAIN DESIGN CRITERIA" DATED AUGUST 1988.
- SIGNATURE
STEVEN PHILLIPS, P.E.

SENIOR MANAGER
WATER RESOURCES

PRINTED NAME AND TITLE
- CERTIFICATION OF THE QUANTITIES
- I HEREBY CERTIFY THAT THE ESTIMATED TOTAL AMOUNT OF EXCAVATION AND FILL AS SHOWN ON THESE PLANS HAS BEEN COMPUTED TO 128 CUBIC YARDS OF EXCAVATION, 11Z CUBIC YARDS OF FILL AND THE TOTAL AREA TO BE DISTURBED AS SHOWN ON THE PLANS HAS BEEN DETERMINED TO BE 43,535 SQUARE FEET.
- SIGNATURE
STEVEN PHILLIPS, P.E.

SENIOR MANAGER
WATER RESOURCES

PRINTED NAME AND TITLE
- DATE
- EN-01
- CITY OF TAKOMA PARK
NEW AVE BIKEWAY, SECTION B
MD 650 (NEW HAMPSHIRE AVENUE)
POPLAR AVE TO AUBURN AVE
SHA TRACKING NO. 20-AP-MO-020-XX
- EROSION AND SEDIMENT CONTROL NOTES
- SCALE NO SCALE DATE NOVEMBER 2021 CONTRACT NO. T.B.D.
- DESIGNED BY NSR

DRAWN BY NSR

CHECKED BY AGB


F.A.P. NO. T.B.D.

COUNTY MONTGOMERY

LOGMILE

WSSC 208NE01
TAX MAPS JN561
- DRAWING NO. EN - 01 OF 3 SHEET NO. 23 OF 44
- PLOTTED: 11/4/2021
FILE: \\ad.rkk.com\its\Cloud\Projects\2020\20007_NewAveSecB\CADD\Plans\pES-N001_NewAveBike_B.dgn

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SEQUENCE OF CONSTRUCTION:

1. PRIOR TO CLEARING TREES, INSTALLING SEDIMENT CONTROL MEASURES, OR GRADING, A PRECONSTRUCTION MEETING MUST BE CONDUCTED ON-SITE WITH THE MONTGOMERY COUNTY DEPARTMENT OF PERMITTING SERVICE (MCDPS) SEDIMENT CONTOL INSPECTOR (240) 777-0311 (48 HOURS NOTICE), THE OWNERS REPRESENTATIVE, AND THE SITE ENGINEER. IN ORDER FOR THE MEETING TO OCCUR, THE APPLICANT MUST PROVIDE ONE PAPER SET OF APPROVED SEDIMENT CONTROL PLANS AND APPROVED ROADSIDE TREE PROTECTION PLAN TO THE MCDPS SEDIMENT CONTROL INSPECTOR AT THE PRECONSTRUCTION MEETING. IF NO PLANS ARE PROVIDED, THE MEETING SHALL NOT OCCUR AND WILL NEED TO BE RESCHEDULED PRIOR TO COMMENCING ANY WORK.
2. LIMIT OF DISTURBANCE MUST BE FIELD MARKED PRIOR TO CLEARING OF TREES, INSTALLATION OF SEDIMENT CONTROL MEASURES, CONSTRUCTION, OR OTHER LAND DISTURBING ACTIVITIES.
3. NO WORK SHALL BE COMPLETED DURING A RAIN EVENT. NO DISTURBED AREA SHALL BE LEFT UNSTABILIZED OVERNIGHT UNLESS THE RUNOFF IS DIRECTED TO AN APPROVED SEDIMENT CONTROL DEVICE. ALL AREAS DESIGNATED AS SAME DAY SHALL BE STABILIZED AT THE END OF EACH WORK DAY.
4. ROOT PRUNE ALONG LOD AT DIRECTION OF MD LTE PRIOR TO ANY WORK BEING PERFORMED.
5. THE PERMITTEE MUST OBTAIN WRITTEN APPROVAL FROM THE MCDPS INSPECTOR, CERTIFYING THAT THE LIMITS OF DISTURBANCE IS CORRECTLY MARKED AND INSTALLED PRIOR TO COMMENCING ANY CLEARING.
6. WITH THE APPROVAL OF THE PROJECT ENGINEER AND THE MCDPS SEDIMENT CONTROL INSPECTOR, STEPS IN EACH STAGE MAY BE ADJUSTED AND/OR BE PERFORMED CONCURRENTLY.
7. WHERE INLET PROTECTION IS USED IN CONJUNCTION WITH SAME-DAY STABILIZATION AND DRAINAGE AREAS EXCEED THE LIMITS REQUIRED, MEASURES SHALL BE TAKEN TO PREVENT THE PROTECTIONS FROM BEING OVERWHELMED WITH SEDIMENT.
8. THE NEED FOR AND LOCATION OF STABILIZED CONSTRUCTION ENTRANCES SHALL BE DISCUSSED AT THE PRECONSTRUCTION MEETING FOR ALL STAGES.
9. UNLESS NEW, ALL CONSTRUCTION MATS SHALL BE POWER WASHED PRIOR TO BEING BROUGHT ON SITE.
- 10.RELOCATE UTILITIES AS NEEDED PRIOR TO COMMENCING WORK.

PHASE 1A: POPLAR AVE TO CONWAY AVE (MD 650 STA. 101+00 TO 103+25)

1. CLEAR AND GRUB FOR INSTALLATION OF SEDIMENT CONTROL DEVICES AND INSTALL THOSE DEVICES SHOWN ON THE EROSION AND SEDIMENT CONTROL PLANS. DURING THIS AND SUBSEQUENT STEPS, SAFE PEDESTRIAN ACCESS MUST BE MAINTAINED AT ALL TIMES.
2. ONCE SEDIMENT CONTROL DEVICES ARE INSTALLED, THE PERMITTEE MUST OBTAIN WRITTEN APPROVAL FROM THE MCDPS INSPECTOR BEFORE PROCEEDING WITH ANY ADDITIONAL CLEARING, GRUBBING, OR GRADING.
3. CONSTRUCT SHARED USE PATH, ALL WIDENING WORK, CURB RECONSTRUCTION, LIGHTING AND SIGNING WORK. USE SAME DAY STABILIZATION IN ALL AREAS NOT DRAINING TO AN APPROVED SEDIMENT CONTROL DEVICE, AS SHOWN ON THE PLANS.
4. ONCE ALL WORK IS COMPLETED AND WITH THE APPROVAL OF THE INSPECTOR, REMOVE EROSION AND SEDIMENT CONTROLS AND PERFORM FINAL STABILIZATION, MOVING ON TO NEXT WORK ZONE.

PHASE 1B: CONWAY AVE TO PRINCE GEORGES AVE (STA. 103+25 TO 105+50)

1. CLEAR AND GRUB FOR INSTALLATION OF SEDIMENT CONTROL DEVICES AND INSTALL THOSE DEVICES SHOWN ON THE EROSION AND SEDIMENT CONTROL PLANS. DURING THIS AND SUBSEQUENT STEPS, SAFE PEDESTRIAN ACCESS MUST BE MAINTAINED AT ALL TIMES.
2. ONCE SEDIMENT CONTROL DEVICES ARE INSTALLED, THE PERMITTEE MUST OBTAIN WRITTEN APPROVAL FROM THE MCDPS INSPECTOR BEFORE PROCEEDING WITH ANY ADDITIONAL CLEARING, GRUBBING, OR GRADING.
3. DURING A NOAA 3-DAY DRY PERIOD, INSTALL INLET I-1 USING SAME DAY STABILIZATION, CONNECTING TO EXISTING PIPE.
4. CONSTRUCT SHARED USE PATH, ALL WIDENING WORK, CURB RECONSTRUCTION, LIGHTING AND SIGNING WORK. USE SAME DAY STABILIZATION IN ALL AREAS NOT DRAINING TO AN APPROVED SEDIMENT CONTROL DEVICE, AS SHOWN ON THE PLANS.
5. ONCE ALL WORK IS COMPLETED AND WITH THE APPROVAL OF THE INSPECTOR, REMOVE EROSION AND SEDIMENT CONTROLS AND PERFORM FINAL STABILIZATION, MOVING ON TO NEXT WORK ZONE.

PHASE 2: PRINCE GEORGES AVE TO BELFORD PL (STA. 105+50 TO 109+50)

1. CLEAR AND GRUB FOR INSTALLATION OF SEDIMENT CONTROL DEVICES AND INSTALL THOSE DEVICES SHOWN ON THE EROSION AND SEDIMENT CONTROL PLANS. INSTALL SILT FENCE FROM STA. 105+58 LT TO 106+96 LT. DURING THIS AND SUBSEQUENT STEPS, SAFE PEDESTRIAN ACCESS MUST BE MAINTAINED AT ALL TIMES.
2. ONCE SEDIMENT CONTROL DEVICES ARE INSTALLED, THE PERMITTEE MUST OBTAIN WRITTEN APPROVAL FROM THE MCDPS INSPECTOR BEFORE PROCEEDING WITH ANY ADDITIONAL CLEARING, GRUBBING, OR GRADING.
3. DURING A NOAA 3-DAY DRY PERIOD, INSTALL INLET I-2 AND PRECAST CONCRETE BIOFILTRATION UNIT USING SAME DAY STABILIZATION, CONNECTING INLET I-2 TO THE BIOFILTRATION UNIT AND THE BIOFILTRATION UNIT TO EXISTING PIPE.
4. CONSTRUCT SHARED USE PATH, ALL WIDENING WORK, CURB RECONSTRUCTION, LIGHTING AND SIGNING WORK. USE SAME DAY STABILIZATION IN ALL AREAS NOT DRAINING TO AN APPROVED SEDIMENT CONTROL DEVICE, AS SHOWN ON THE PLANS.
5. ONCE ALL WORK IS COMPLETED AND WITH THE APPROVAL OF THE INSPECTOR, REMOVE EROSION AND SEDIMENT CONTROLS AND PERFORM FINAL STABILIZATION, MOVING ON TO NEXT WORK ZONE.

PHASE 3: BELFORD PL TO ETHAN ALLEN AVE (STA. 109+50 TO 120+50)

1. CLEAR AND GRUB FOR INSTALLATION OF SEDIMENT CONTROL DEVICES AND INSTALL THOSE DEVICES SHOWN ON THE EROSION AND SEDIMENT CONTROL PLANS. DURING THIS AND SUBSEQUENT STEPS, SAFE PEDESTRIAN ACCESS MUST BE MAINTAINED AT ALL TIMES.
2. ONCE SEDIMENT CONTROL DEVICES ARE INSTALLED, THE PERMITTEE MUST OBTAIN WRITTEN APPROVAL FROM THE MCDPS INSPECTOR BEFORE PROCEEDING WITH ANY ADDITIONAL CLEARING, GRUBBING, OR GRADING.
3. DURING A NOAA 3-DAY DRY PERIOD, INSTALL INLETS I-3 AND I-4 AND 18" RCP CONNECTING INLETS I-3 AND I-4 USING SAME DAY STABILIZATION. CONNECT INLET I-4 TO EXISTING PIPE.
4. CONSTRUCT SHARED USE PATH, ALL WIDENING WORK, CURB RECONSTRUCTION, LIGHTING AND SIGNING WORK. USE SAME DAY STABILIZATION IN ALL AREAS NOT DRAINING TO AN APPROVED SEDIMENT CONTROL DEVICE, AS SHOWN ON THE PLANS.
5. ONCE ALL WORK IS COMPLETED AND WITH THE APPROVAL OF THE INSPECTOR, REMOVE EROSION AND SEDIMENT CONTROLS AND PERFORM FINAL STABILIZATION, MOVING ON TO NEXT WORK ZONE.

PHASE 4: ETHAN ALLEN AVE TO AUBURN AVE (STA. 120+50 TO 123+25)

1. CLEAR AND GRUB FOR INSTALLATION OF SEDIMENT CONTROL DEVICES AND INSTALL THOSE DEVICES SHOWN ON THE EROSION AND SEDIMENT CONTROL PLANS. DURING THIS AND SUBSEQUENT STEPS, SAFE PEDESTRIAN ACCESS MUST BE MAINTAINED AT ALL TIMES.
2. ONCE SEDIMENT CONTROL DEVICES ARE INSTALLED, THE PERMITTEE MUST OBTAIN WRITTEN APPROVAL FROM THE MCDPS INSPECTOR BEFORE PROCEEDING WITH ANY ADDITIONAL CLEARING, GRUBBING, OR GRADING.
3. DURING A NOAA 3-DAY DRY PERIOD, INSTALL INLET I-5 AND MH-1 USING SAME DAY STABILIZATION, CONNECTING I-5 TO MH-1 AND EXISTING PIPES TO MH-1 REMOVING SEGMENTS OF EXISTING PIPE AS NECESSARY.
4. CONSTRUCT SHARED USE PATH, ALL WIDENING WORK, CURB RECONSTRUCTION, LIGHTING AND SIGNING WORK. USE SAME DAY STABILIZATION IN ALL AREAS NOT DRAINING TO AN APPROVED SEDIMENT CONTROL DEVICE, AS SHOWN ON THE PLANS.
5. ONCE ALL WORK IS COMPLETED AND WITH THE APPROVAL OF THE INSPECTOR, REMOVE EROSION AND SEDIMENT CONTROLS AND PERFORM FINAL STABILIZATION, MOVING ON TO NEXT WORK ZONE.

EN-02

CITY OF TAKOMA PARK
NEW AVE BIKEWAY, SECTION B
MD 650 (NEW HAMPSHIRE AVENUE)
POPLAR AVE TO AUBURN AVE
SHA TRACKING NO. 20-AP-MO-020-XX

EROSION AND SEDIMENT CONTROL NOTES

SCALE __ NO SCALE __ DATE __ NOVEMBER 2021 __ CONTRACT NO. __ T.B.D. __

DESIGNED BY __ NSR __ COUNTY __ MONTGOMERY __
DRAWN BY __ NSR __ LOGMILE __
CHECKED BY __ AGB __
F.A.P. NO. __ T.B.D. __ WSSC 208NE01
TAX MAPS JN561

DRAWING NO. EN – 02 OF 3 SHEET NO. 24 OF 44

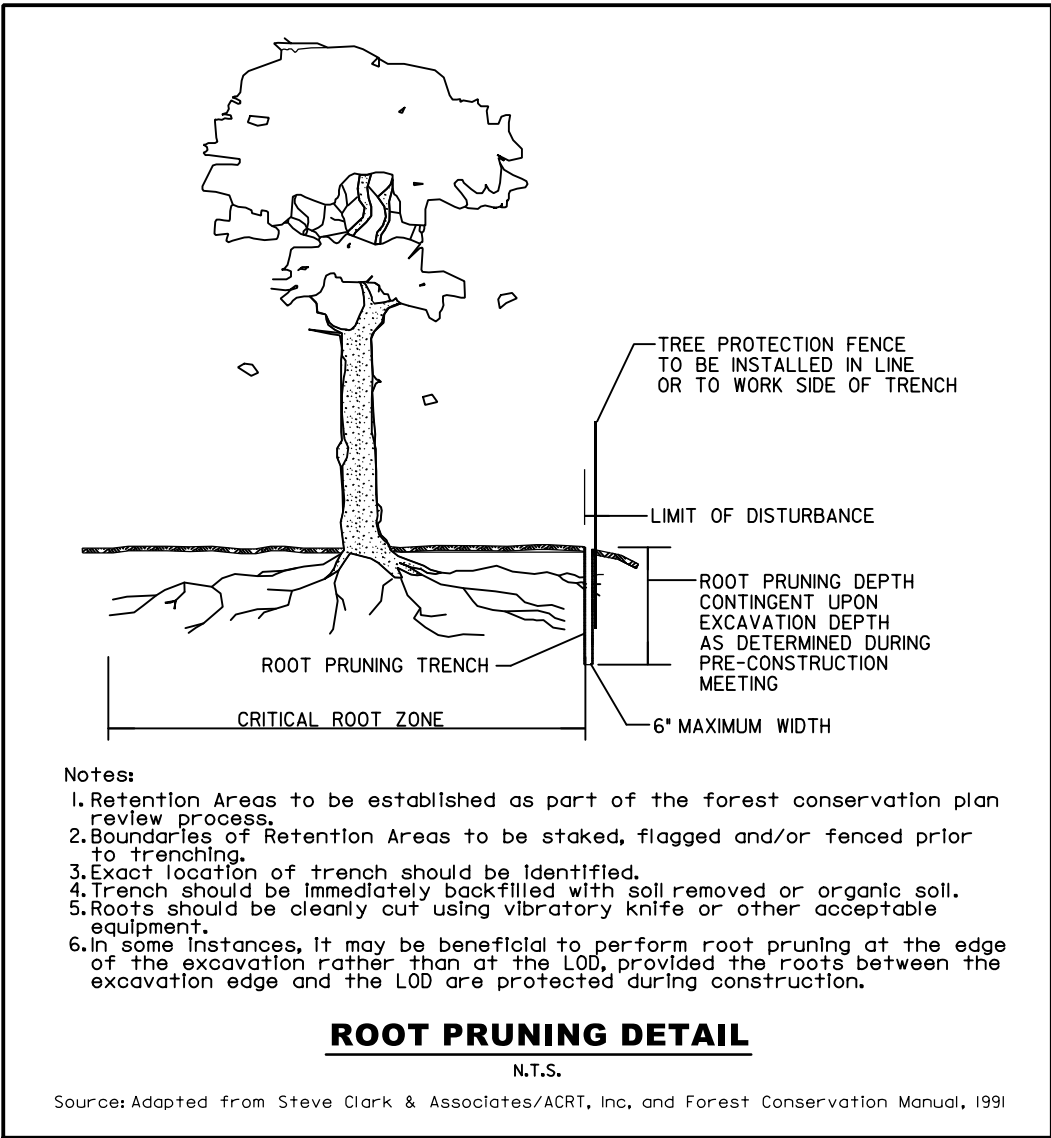
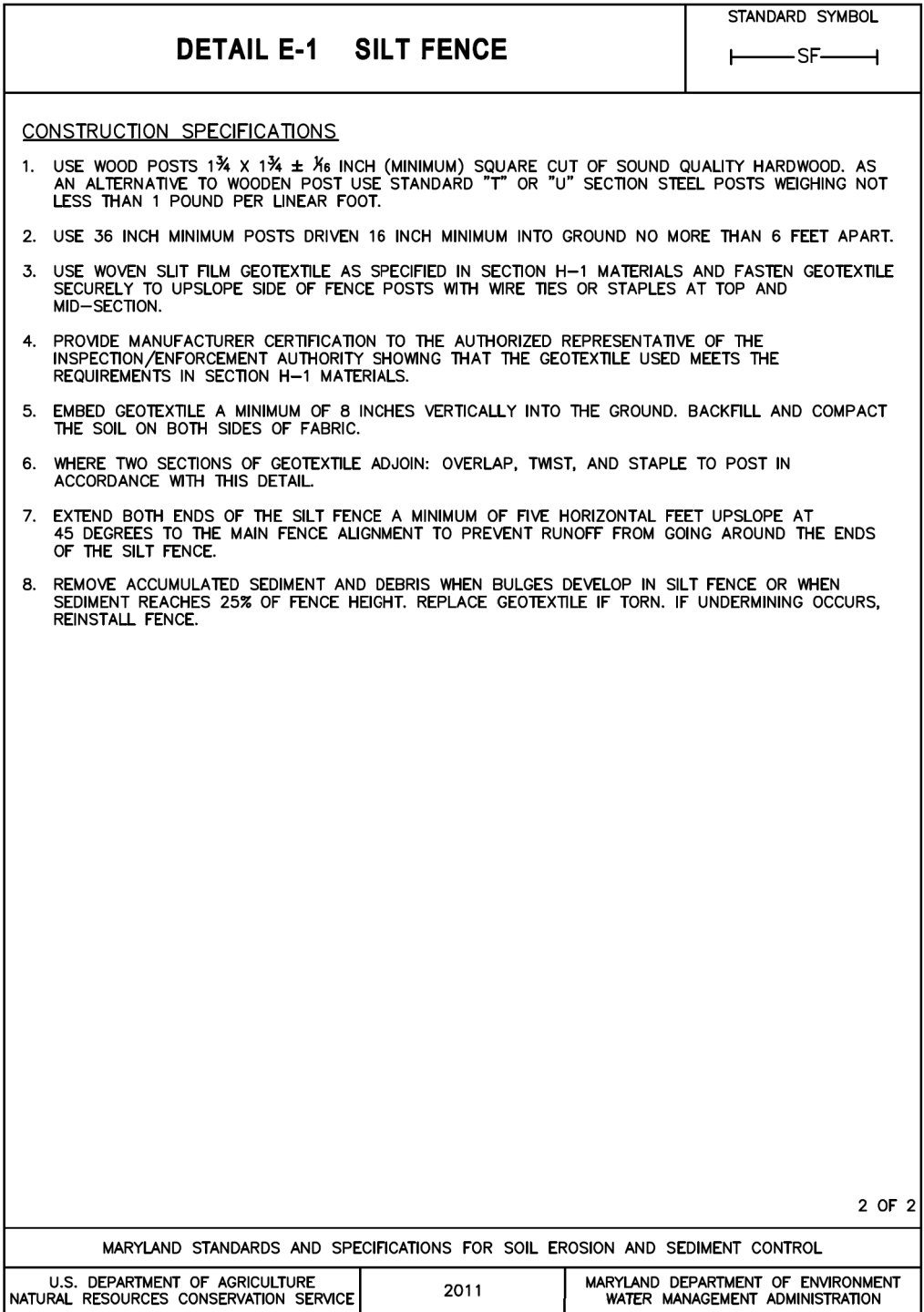
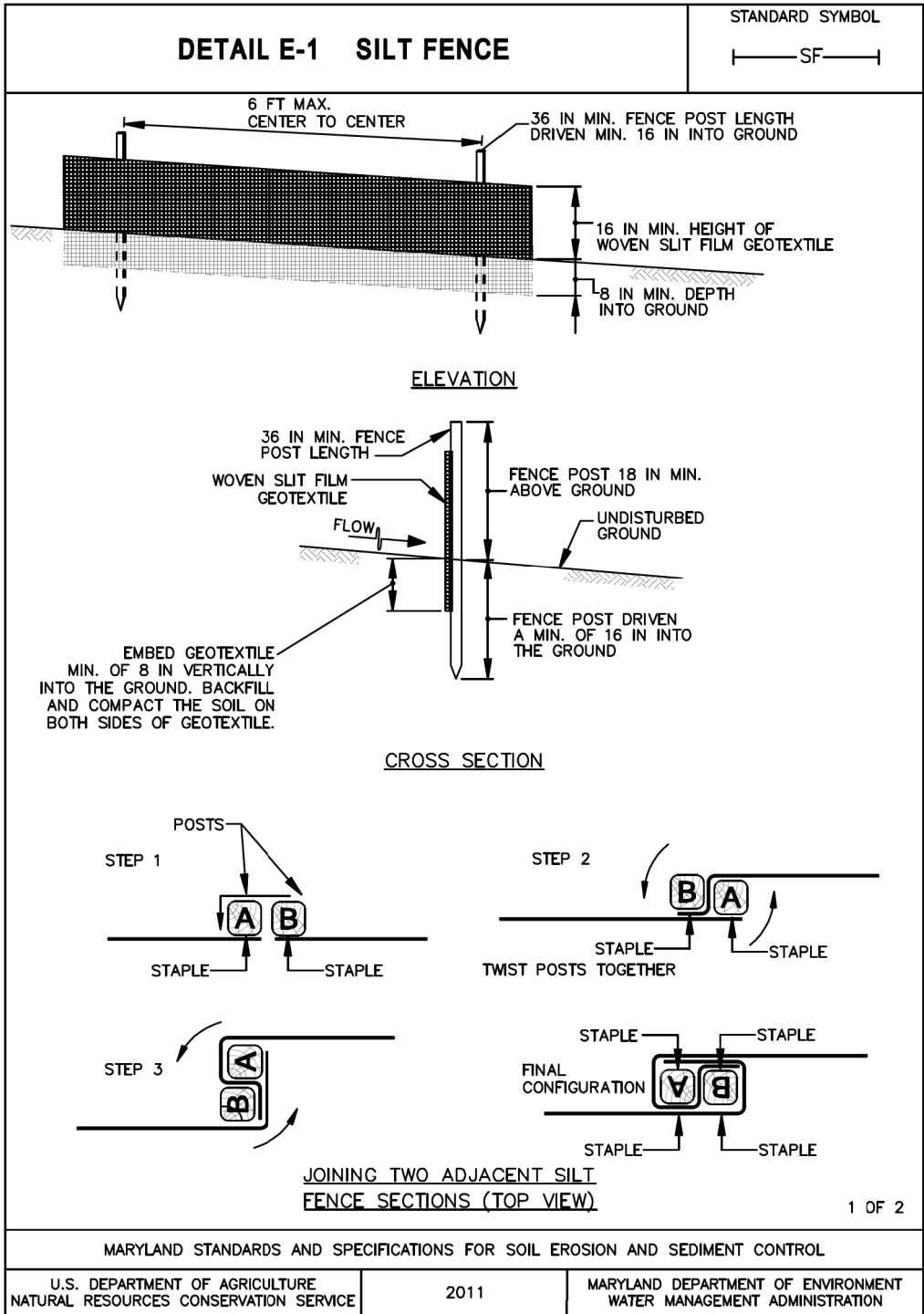
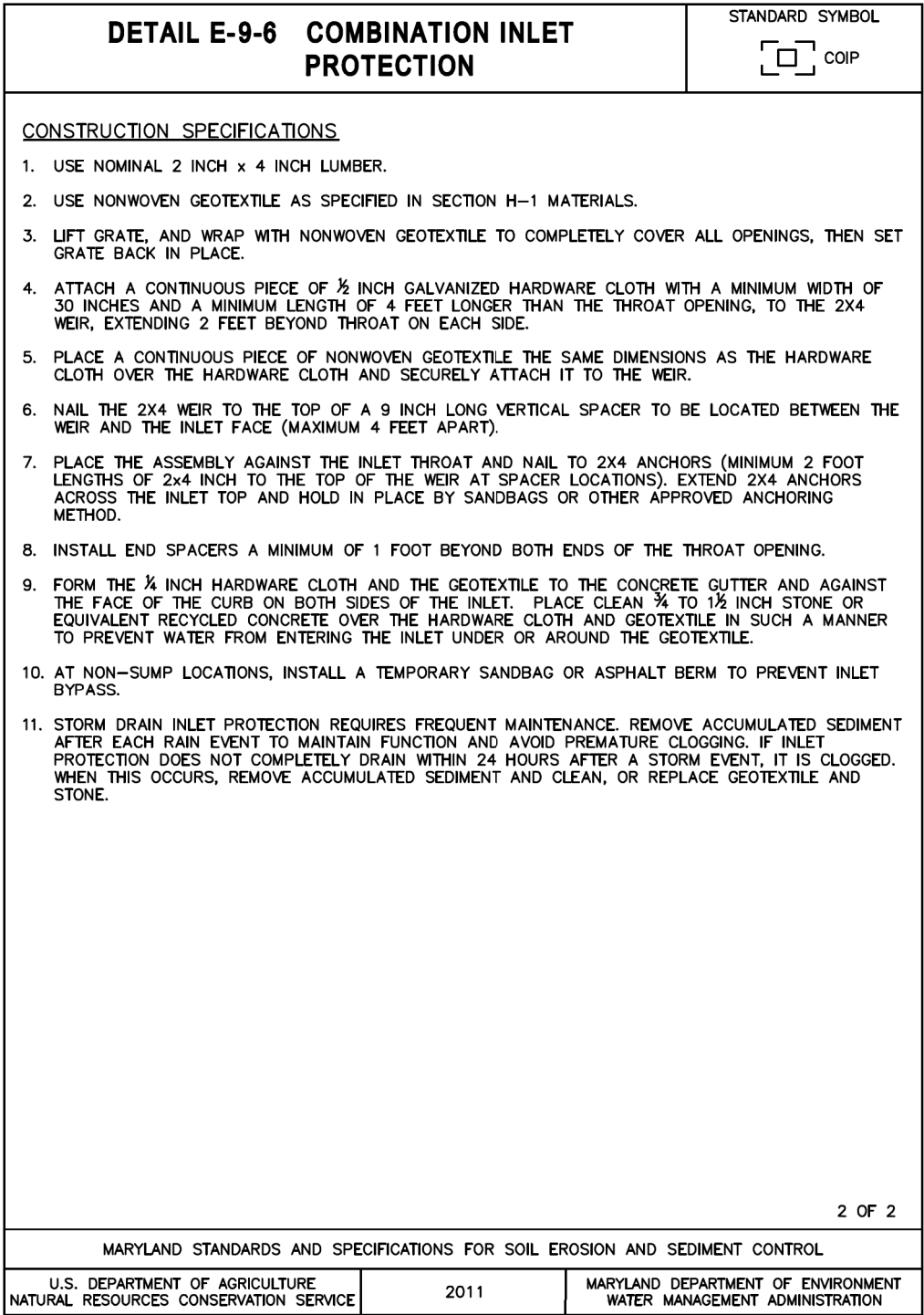
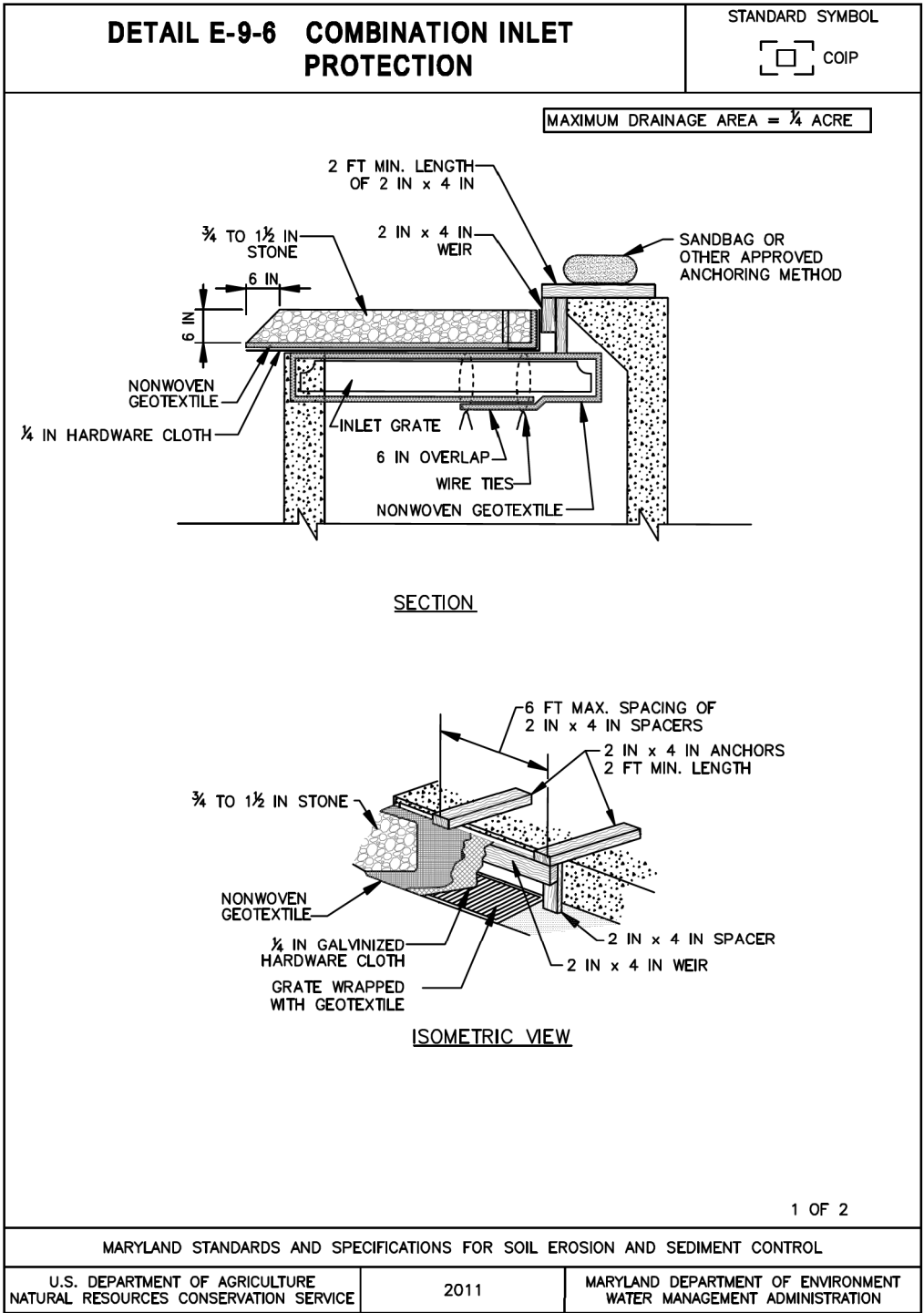
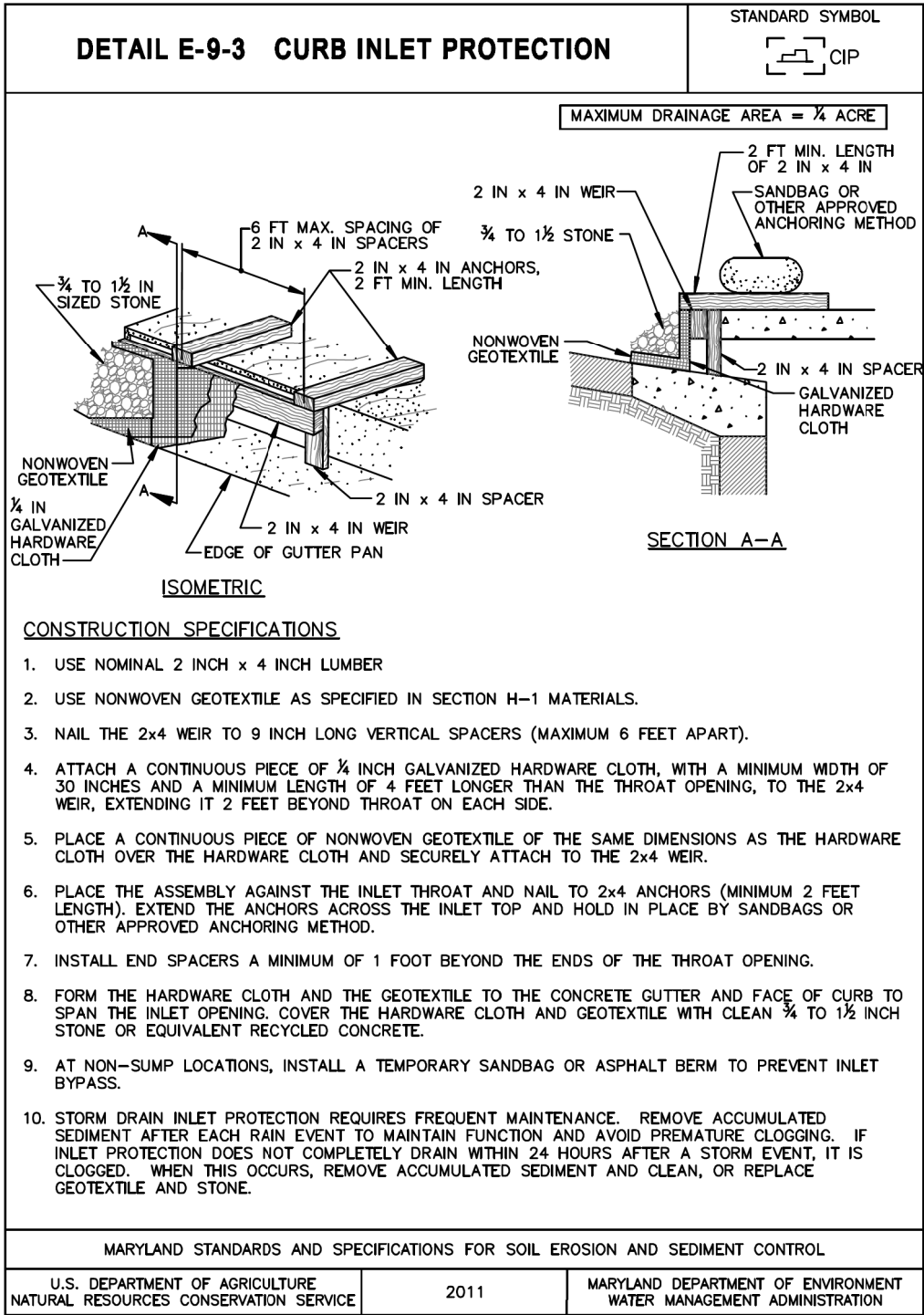
PLOTTED: 11/4/2021
FILE: \\ad.rkk.com\its\Cloud\Projects\2020\20007_NewAveSecB\CADD\Plans\pES-N002_NewAveBike_B.dgn



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EN-03

CITY OF TAKOMA PARK
NEW AVE BIKEWAY, SECTION B
MD 650 (NEW HAMPSHIRE AVENUE)
POPLAR AVE TO AUBURN AVE
SHA TRACKING NO. 20-AP-MO-020-XX

EROSION AND SEDIMENT CONTROL DETAILS

SCALE NO SCALE DATE NOVEMBER 2021 CONTRACT NO. T.B.D.

DESIGNED BY NSR COUNTY MONTGOMERY
DRAWN BY NSR LOGMILE
CHECKED BY AGB
F.A.P. NO. T.B.D. WSSC 208NE01
TAX MAPS JN561

DRAWING NO. EN - 03 OF 3 SHEET NO. 25 OF 44

PLOTTED: 10/28/2021
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BY: kboampong -



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LEGEND

- 5 INCH CONCRETE SIDEWALK
- CONCRETE DRIVEWAY
- ASPHALT SHARED USE PATH
- FULL DEPTH ASPHALT PAVING
- SIDEWALK AND PAVEMENT REMOVAL (PAID FOR AS CLASS 1 EXCAVATION)
- SAME DAY STABILIZATION
- SWM FOOTPRINT

- LOD LIMIT OF DISTURBANCE
- SF SILT FENCE
- CIP INLET PROTECTION
- COMBINATION INLET PROTECTION

NOTES:

- WHERE NO SCE IS PROVIDED, CONTRACTOR SHALL DESIGNATE PIECES OF CONSTRUCTION EQUIPMENT WITHIN THE LOD. THIS EQUIPMENT SHALL BE KEPT WITHIN THE LOD UNTIL THE PROPOSED WORK IS COMPLETE AND SHALL HAVE TREADS/TIRES CLEANED PRIOR TO LEAVING THE LOD.
- ALL STAGING AND STOCKPILING MUST BE WITHIN THE LOD. SEDIMENT CONTROL FOR THE STAGING STOCKPILE AREA MUST BE PROVIDED IF NEEDED OR AS DIRECTED BY THE SEDIMENT CONTROL INSPECTOR.



NO.	REVISION	DATE	BY

PLOTTED: 10/26/2021
FILE: \\ad.rkk.com\ts\Cloud\Projects\2020\20007_NewAveSecB\CADD\Plans\pES-0001_NewAveBike_B.dgn

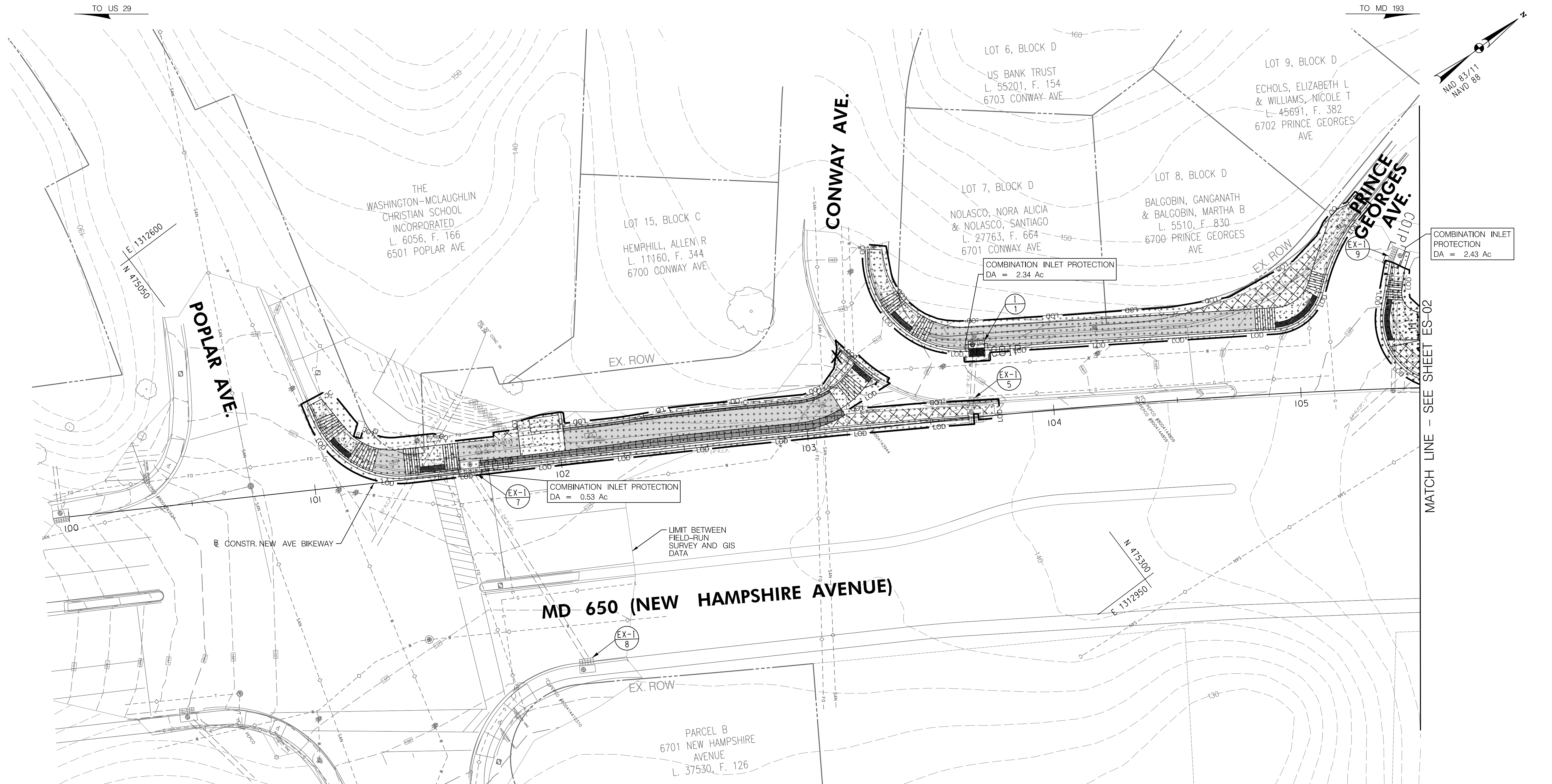
CITY OF TAKOMA PARK
NEW AVE BIKEWAY, SECTION B
MD 650 (NEW HAMPSHIRE AVENUE)
POPLAR AVE TO AUBURN AVE
SHA TRACKING NO. 20-AP-MO-020-XX

EROSION AND SEDIMENT CONTROL PLAN

SCALE 1"=20' DATE NOVEMBER 2021 CONTRACT NO. T.B.D.

DESIGNED BY NSR COUNTY MONTGOMERY
DRAWN BY NSR LOGMILE
CHECKED BY AGB
F.A.P. NO. T.B.D. WSSC 208NE01
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DRAWING NO. ES - 01 OF 4 SHEET NO. 26 OF 44



ES-01

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LEGEND

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20' 0 20' 40'
SCALE: 1" = 20'

NO.	REVISION	DATE	BY

PLOTTED: 10/26/2021
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CITY OF TAKOMA PARK
NEW AVE BIKEWAY, SECTION B
MD 650 (NEW HAMPSHIRE AVENUE)
POPLAR AVE TO AUBURN AVE
SHA TRACKING NO. 20-AP-MO-020-XX

EROSION AND SEDIMENT CONTROL PLAN

SCALE 1"=20' DATE NOVEMBER 2021 CONTRACT NO. T.B.D.

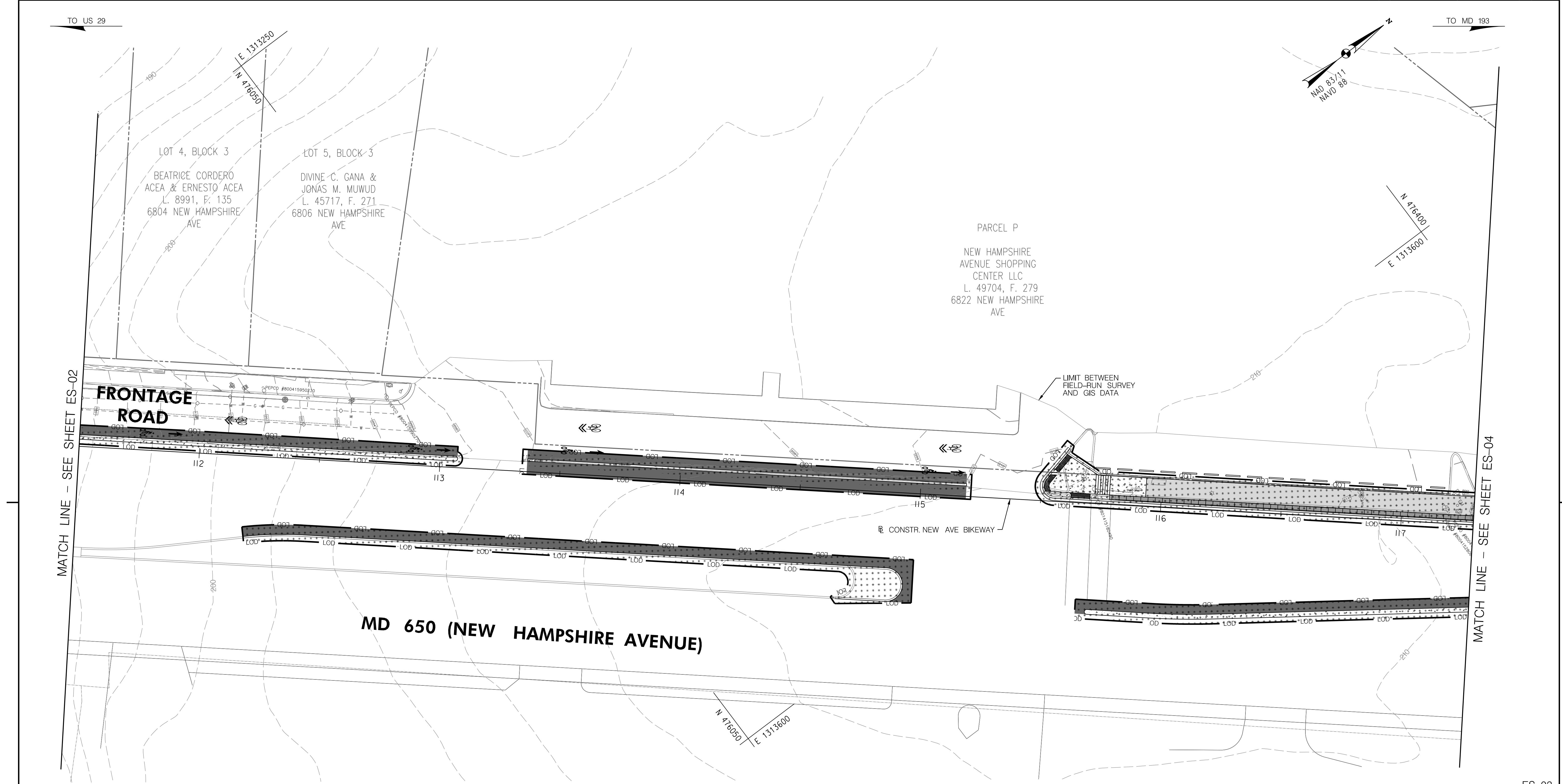
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F.A.P. NO. T.B.D. WSSC 208NE01
TAX MAPS JN561

DRAWING NO. ES - 02 OF 4 SHEET NO. 27 OF 44

MATCH LINE - SEE SHEET ES-01

MATCH LINE - SEE SHEET ES-03

ES-02



ES-03

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LEGEND

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- CONCRETE DRIVEWAY
- ASPHALT SHARED USE PATH
- FULL DEPTH ASPHALT PAVING
- SIDEWALK AND PAVEMENT REMOVAL
(PAID FOR AS CLASS 1 EXCAVATION)
- SAME DAY STABILIZATION
- SWM FOOTPRINT
- LIMIT OF DISTURBANCE
- SILT FENCE
- INLET PROTECTION
- COMBINATION INLET PROTECTION

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SCALE: 1" = 20'

NO.	REVISION	DATE	BY

CITY OF TAKOMA PARK
NEW AVE BIKEWAY, SECTION B
MD 650 (NEW HAMPSHIRE AVENUE)
POPLAR AVE TO AUBURN AVE
SHA TRACKING NO. 20-AP-MO-020-XX

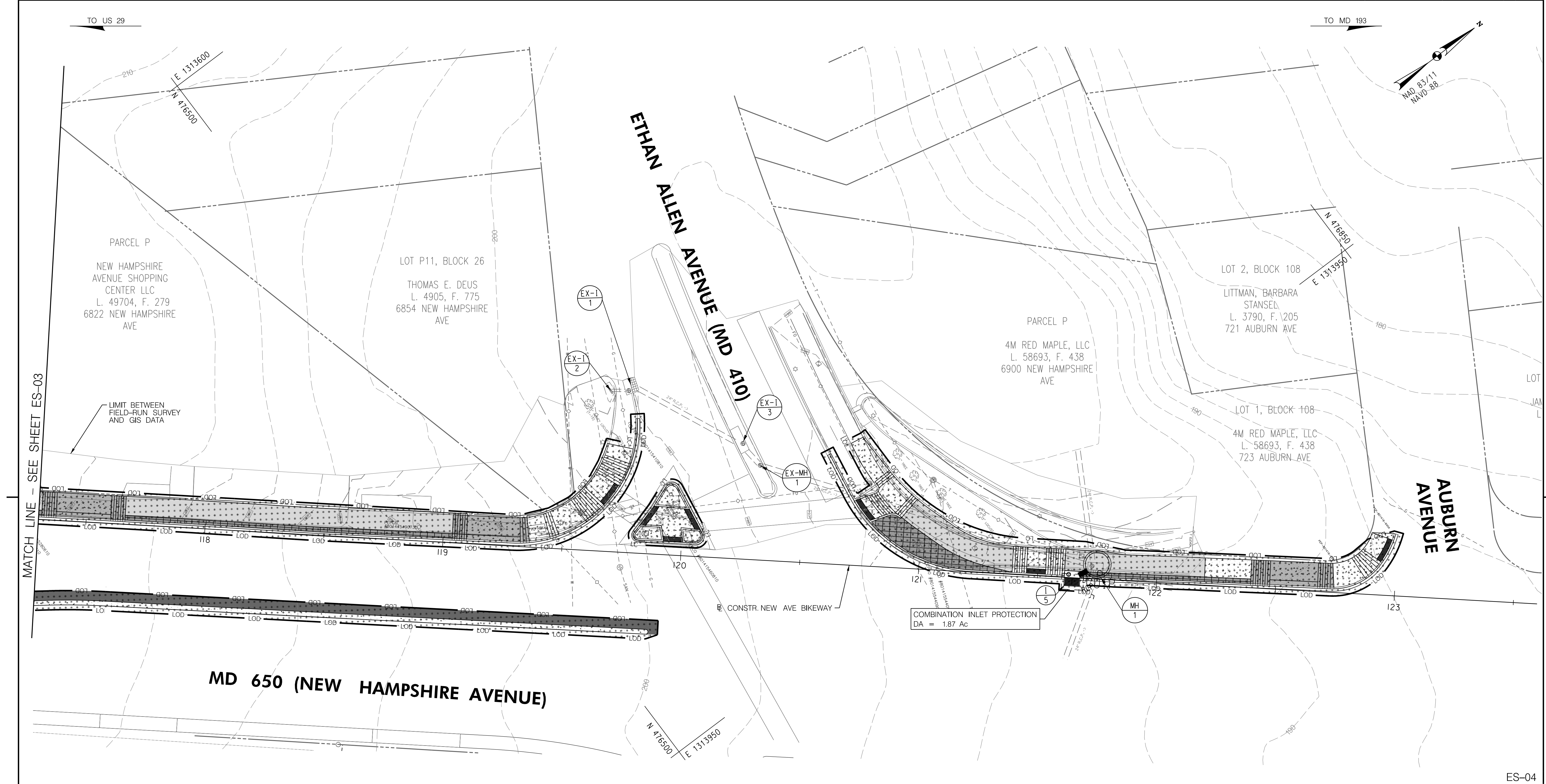
EROSION AND SEDIMENT CONTROL PLAN

SCALE 1"=20' DATE NOVEMBER 2021 CONTRACT NO. T.B.D.

DESIGNED BY	NSR	COUNTY	MONTGOMERY
DRAWN BY	NSR	LOGMILE	
CHECKED BY	AGB		
F.A.P. NO.	T.B.D.	WSSC 208NE01	TAX MAPS JN561

DRAWING NO.	ES - 03	OF	4	SHEET NO.	28	OF	44
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ES-04

BY: kboampong -





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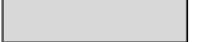
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LEGEND

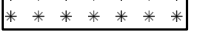
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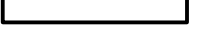
 CONCRETE DRIVEWAY


 ASPHALT SHARED USE PATH

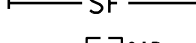
 FULL DEPTH ASPHALT PAVING

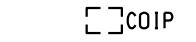
 SIDEWALK AND PAVEMENT REMOVAL (PAID FOR AS CLASS 1 EXCAVATION)


 SAME DAY STABILIZATION


 SWM FOOTPRINT

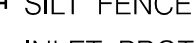
 LOD


 SF


 CIP

 CIP

 LIMIT OF DISTURBANCE

 SILT FENCE

 INLET PROTECTION

 COMBINATION INLET PROTECTION

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20'

0

20'

40'

SCALE: 1" = 20'

NO.	REVISION	DATE	BY

CITY OF TAKOMA PARK
NEW AVE BIKEWAY, SECTION B
MD 650 (NEW HAMPSHIRE AVENUE)
POPLAR AVE TO AUBURN AVE
SHA TRACKING NO. 20-AP-MO-020-XX

EROSION AND SEDIMENT CONTROL PLAN

SCALE: 1"=20' DATE: NOVEMBER 2021 CONTRACT NO.: T.B.D.

DESIGNED BY: NSR

DRAWN BY: NSR

CHECKED BY: AGB

F.A.P. NO.: T.B.D.

COUNTY: MONTGOMERY

LOGMILE:

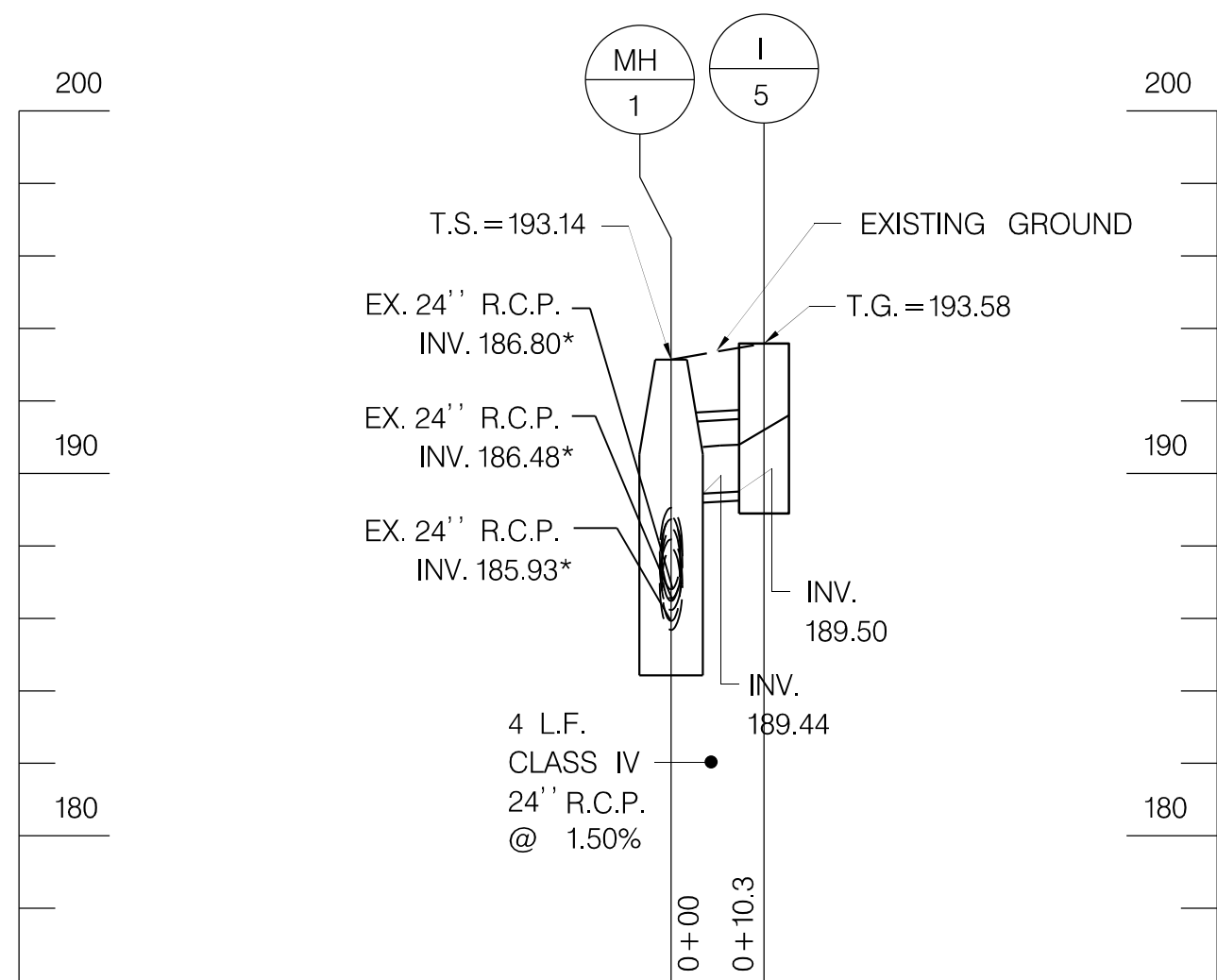
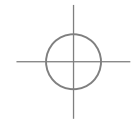
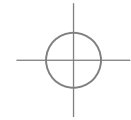
WSSC 208NE01
TAX MAPS JN561

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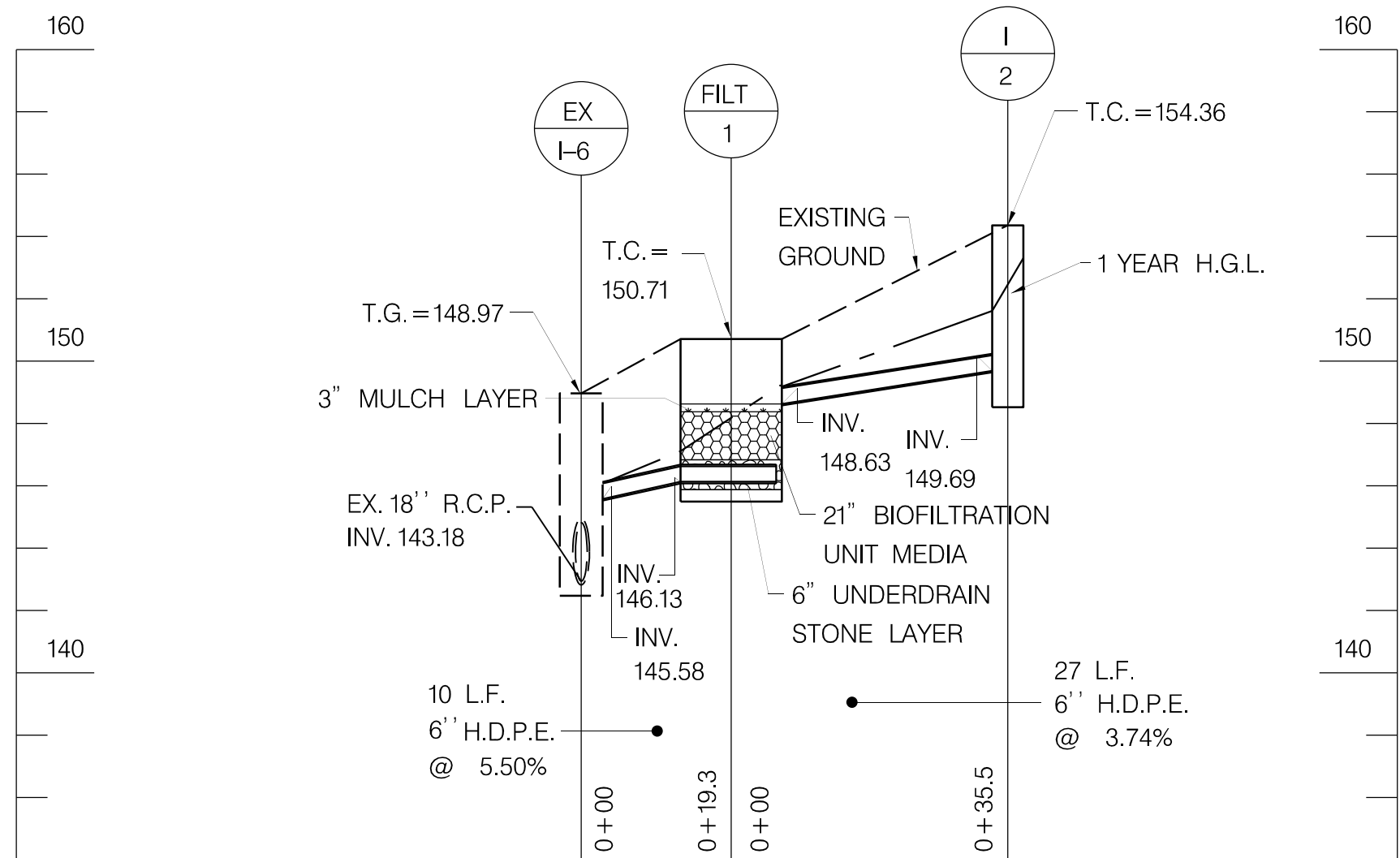
OF 4

SHEET NO. 29 OF 44

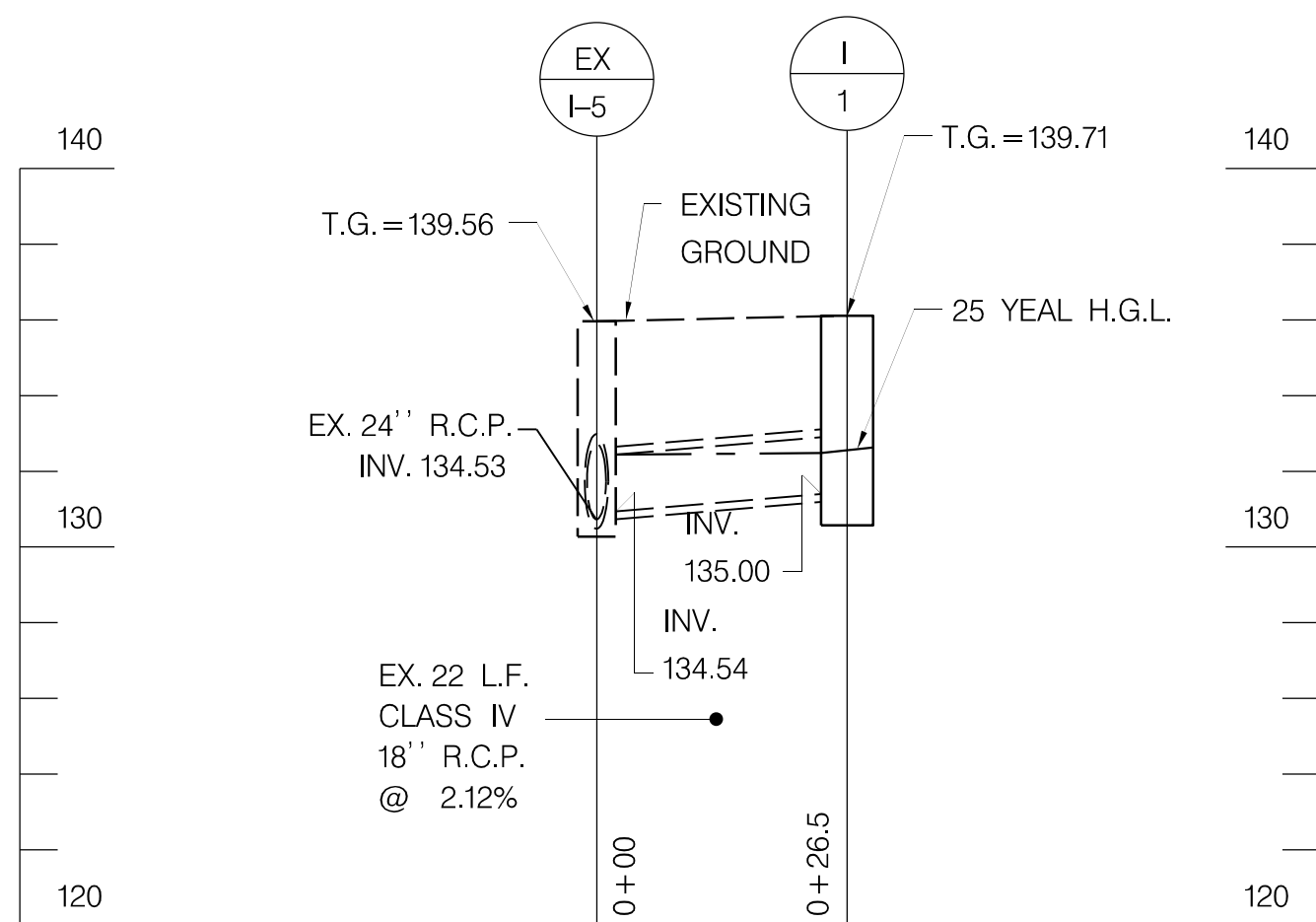
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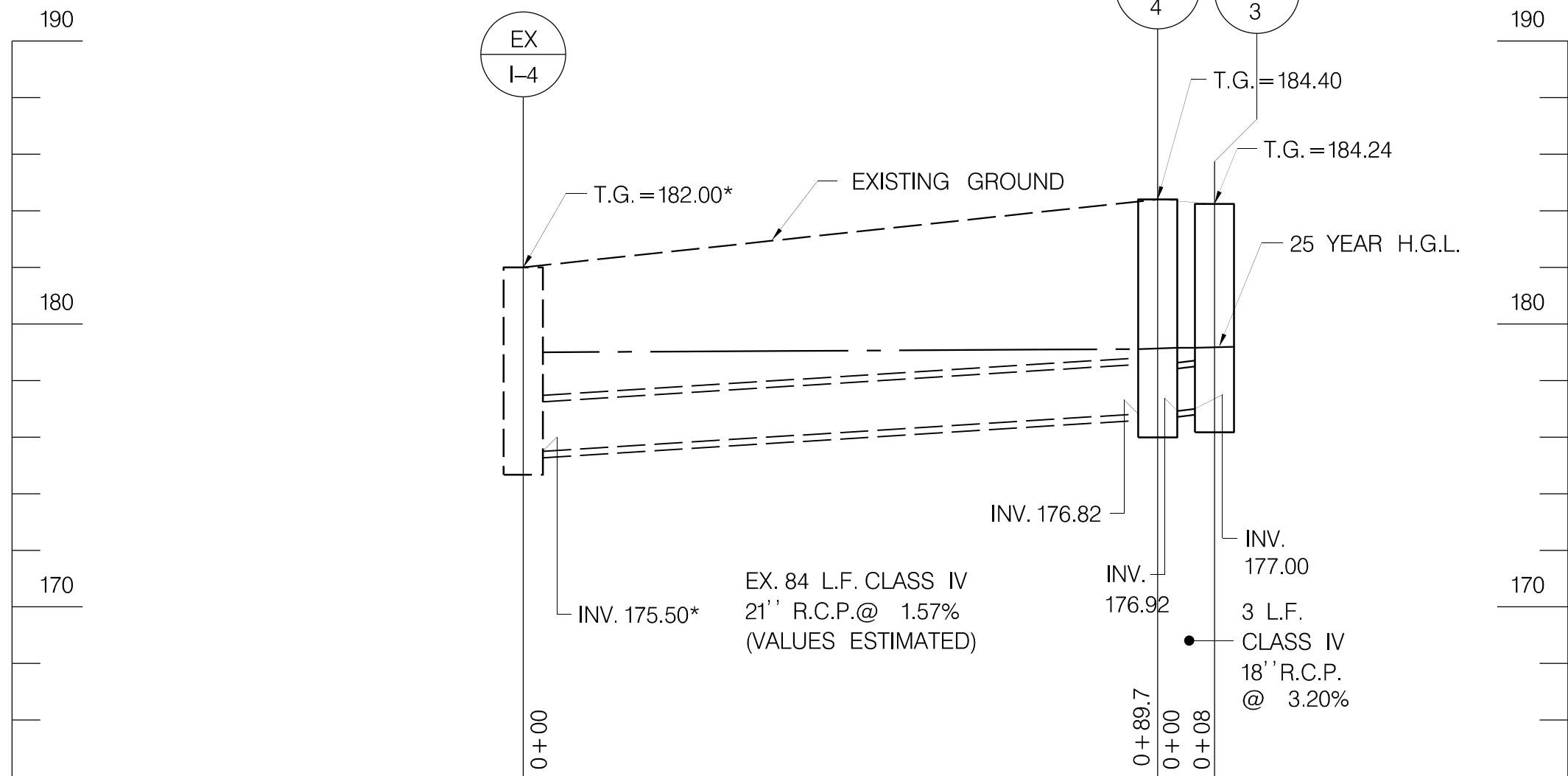
I-5 TO MH-1



I-2 TO EX-I-6



I-1 TO EX-I-5



I-3 TO EX-I-4

STRUCTURE SCHEDULE

NAME	STATION	OFFSET (FT)	INVERT OUT (FT)	VERTICAL DEPTH (FT)	TYPE	STANDARD
I-1	103+70	27.7 LT	135.00'	1.2	PRECAST WRM INLET	MD-374.22
I-2	106+25	0.0 LT	149.69'	0.0	STANDARD COG INLETS 5'	MD-374.31
I-3	110+11	7.0 LT	177.00'	3.7	PRECAST STANDARD TYPE S INLET DOUBLE GRATE TANDEM	MD-374.70
I-4	110+11	0.0 LT	176.82'	4.1	PRECAST STANDARD TYPE S INLET DOUBLE GRATE TANDEM	MD-374.70
I-5	121+65	0.0 LT	189.50'	0.6	PRECAST WRM INLET	MD-374.22
MH-I	121+75	6.6 LT	185.93'	0.0	84" DIAMETER PRECAST MANHOLE FOR 54" & 60" PIPES	MD-384.07
FILT-1	105+89	3.3 LT	146.13'	--	13X7 BIOFILTRATION UNIT; SEE SHEET SW-01 FOR DETAILS	--

PIPE SCHEDULE

UPSTREAM STRUCTURE	DOWNSTREAM STRUCTURE	TYPE	LENGTH	EXISTING/PROPOSED
I-5	MH-1	24" RCP	4 LF	PROPOSED
I-3	I-4	18" RCP	3 LF	PROPOSED
I-2	FILT-1	6" HDPE	27 LF	PROPOSED
FILT-1	EX-I-6	6" HDPE	10 LF	PROPOSED

* ELEVATIONS ESTIMATE BASED ON AERIAL IMAGERY/GIS INFORMATION/STANDARD MINIMUM DEPTHS. CONTRACTOR IS TO VERIFY ALL ELEVATIONS BEFORE ORDERING STRUCTURES.

NOTE:
INFLOW/OUTFLOW PIPES TO FILT-1 DESIGNED TO CONTAIN 1-YR DESIGN STORM AND PROVIDE WQV ONLY WITHIN FACILITY. LARGER STORM FLOWS WILL BYPASS FACILITY TO EX-I-6.

SCALE:
HORIZONTAL: 1"=20'
VERTICAL: 1"=4'

DP-01

CITY OF TAKOMA PARK
NEW AVE BIKEWAY, SECTION B
MD 650 (NEW HAMPSHIRE AVENUE)
POPLAR AVE TO AUBURN AVE
SHA TRACKING NO. 20-AP-MO-020-XX

DRAINAGE PROFILES

SCALE SEE SHEET DATE NOVEMBER 2021 CONTRACT NO. T.B.D.

DESIGNED BY AGB COUNTY MONTGOMERY
DRAWN BY AGB LOGMILE
CHECKED BY SBP
F.A.P. NO. T.B.D.

DRAWING NO. DP-01 OF 01 SHEET NO. 30 OF 44

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BY: sbarefoot -

1. MAINTAIN ACCESS TO ALL ROADWAYS, FRONTAGE ROADS, DRIVEWAY ENTRANCES AND ON-STREET PARKING AT ALL TIMES UNLESS DIRECTED OTHERWISE BY THE ENGINEER. CLOSURE OF DRIVEWAY ENTRANCES, FRONTAGE ROADS AND ON-STREET PARKING MUST BE COORDINATED WITH THE PROPERTY OWNERS THROUGH THE CITY OF TAKOMA PARK AND THE ENGINEER.
2. ALL STANDARD REGULATORY AND WARNING SIGNS USED FOR MAINTENANCE OF TRAFFIC SHALL CONFORM TO THE LATEST VERSION OF FHWA'S "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", AS WELL AS MDOT SHA'S "BOOK OF STANDARDS" AND "SUPPLEMENT TO MUTCD".
3. CONSTRUCTION EQUIPMENT AND MATERIALS SHALL BE STORED OFF THE TRAVEL LANES AND PEDESTRIAN FACILITIES AT ALL TIMES.
4. EXISTING REGULATORY SIGNS IN THE WORK ZONE SHALL BE MAINTAINED AT ALL TIMES AS DIRECTED BY THE ENGINEER. SIGNS THAT ARE NOT APPLICABLE SHALL BE REMOVED OR COMPLETELY COVERED WITH NONTRANSPARENT MATERIAL.
5. REFER TO SP 104 FOR WORK RESTRICTIONS AND TEMPORARY LANE CLOSURE SCHEDULE. WORK IS NOT PERMITTED ON SATURDAYS OR SUNDAYS, WITHOUT ADVANCE NOTICE AND WRITTEN PERMISSION FROM THE CITY OF TAKOMA PARK.
6. WHERE TRAVEL LANES ARE ADJACENT TO THE WORKZONE, MAINTAIN A MINIMUM LANE WIDTH OF 10' ALONG MD 650 (NEW HAMPSHIRE AVE) AND A MINIMUM LANE WIDTH OF 9' ALONG ONE-WAY FRONTAGE ROADS. PARKING LANES SHALL BE MAINTAINED AT A MINIMUM OF 7' WIDE.
7. NOTIFY THE WASHINGTON METROPOLITAN TRANSIT AUTHORITY (WMATA) AND THE PASSENGER FACILITIES MANAGER WITH MONTGOMERY COUNTY RIDE-ON, TWO WEEKS IN ADVANCE OF ANY IMPACTS TO EXISTING BUS STOPS WITHIN THE PROJECT LIMITS.
8. MAINTAIN POSITIVE DRAINAGE ALONG THE ROADWAY SURFACE THROUGHOUT CONSTRUCTION.
9. MISS UTILITY SHALL BE NOTIFIED AT LEAST 48 HOURS PRIOR TO ANY EXCAVATION.
10. THE SEQUENCE OF CONSTRUCTION IS PROVIDED FOR THE CONTRACTOR'S USE AND CONSIDERATION. THE SEQUENCE OF CONSTRUCTION MAY BE MODIFIED BY THE CONTRACTOR WITH PRIOR APPROVAL BY THE CITY OF TAKOMA PARK.
11. COORDINATE CONSTRUCTION ACTIVITIES WITH PEPSCO, WHO WILL PERFORM UTILITY POLE RELOCATIONS PRIOR TO CONSTRUCTION.

THE FOLLOWING TTCTA FROM THE SHA BOOK OF STANDARDS ARE TO BE FOLLOWED AS APPROPRIATE:	
MD 104.03-10	-INTER. FAR-LEFT LANE CLOSURE /MULTILANE UNDIV.EQL /LESS THAN 40 MPH
MD 104.03-12	-INTER. FAR-RIGHT LANE CLOSURE /MULTILANE UNDIV.EQL /LESS THAN 40 MPH
MD 104.04-04	-LEFT LANE CLOSURE /DIVDED UNCON.EQL /LESS THAN 40 MPH
MD 104.04-06	-RIGHT LANE CLOSURE /DIVDED UNCON.EQL /LESS THAN 40 MPH
MD 104.04-16	-INTER. (LEFT LANE, TURN BAY) CLOSURE /DIVDED UNCON.EQL /LESS THAN 40 MPH
MD 104.06-01 TO MD 104.06-04	-INSTALLING AND REMOVING CLOSURE SETUPS
MD 104.06-09A AND MD 104.06-09C	-PEDESTRIAN AND CURB LANE CONTROL

GENERAL

1. PRIOR TO CONSTRUCTION, FIELD MARK THE LIMITS OF DISTURBANCE AND OBTAIN WRITTEN APPROVAL FROM THE CITY OF TAKOMA PARK INSPECTOR.
2. SET TEMPORARY TRAFFIC CONTROL DEVICES AS INDICATED IN THE BELOW SEQUENCE.
3. INSTALL EROSION AND SEDIMENT CONTROL DEVICES IN ACCORDANCE WITH THE EROSION AND SEDIMENT CONTROL PLANS.
4. WORK SHALL NOT PROCEED AHEAD TO THE NEXT PHASE UNTIL ALL DISTURBED AREAS ARE STABILIZED. ALL WORK SHALL BE COMPLETED FOR A PHASE BEFORE PROCEEDING TO THE NEXT PHASE.

1. SET TEMPORARY TRAFFIC CONTROL DEVICES ALONG SOUTHBOUND MD 650 FOLLOWING MD 104.04-06.
2. REMOVE EXISTING PEDESTAL POLE AND TRANSFORMER BASE. INSTALL PEDESTAL POLE WITH RELOCATED APSPCS. RELOCATE EXISTING PUSH BUTTON. COORDINATE RELOCATION OF EXISTING BUS STOP SHELTER, BENCH, AND BUS STOP SIGN WITH WMATA AND MONTGOMERY COUNTY RIDE-ON. CONSTRUCT CURB & GUTTER, BACKING CURB, ASPHALT SHARED USE PATH, TYPE 2 PAVERS, CONCRETE SIDEWALK, CONCRETE PEDESTRIAN RAMPS, AND DETECTABLE WARNING SURFACES.

PUSH BUTTON SIGNALS SHALL BE REACTIVATED PRIOR TO ADVANCING TO THE NEXT PHASE OF WORK.
3. INSTALL PERMANENT SIGNING & PAVEMENT MARKINGS AS SHOWN ON THE PLANS.

1. REMOVE PHASE 1A TEMPORARY TRAFFIC CONTROL DEVICES AND SIGNS THAT ARE NO LONGER NEEDED.
2. CLOSE FRONTAGE ROAD.
3. REMOVE EXISTING INLET AND REPLACE WITH INLET I-1. RELOCATE EXISTING FIRE HYDRANT. CONSTRUCT CURB & GUTTER, ASPHALT SHARED USE PATH, CONCRETE PEDESTRIAN RAMPS, AND DETECTABLE WARNING SURFACES.
4. INSTALL PERMANENT SIGNING & PAVEMENT MARKINGS AS SHOWN ON THE PLANS.

1. REMOVE PHASE 1B TEMPORARY TRAFFIC CONTROL DEVICES AND SIGNS THAT ARE NO LONGER NEEDED.
2. SET TEMPORARY TRAFFIC CONTROL DEVICES ALONG SOUTHBOUND MD 650 FOLLOWING MD 104.04-06.
3. INSTALL PRECAST CONCRETE BIOFILTRATION UNIT AND INLET I-2 AS INDICATED ON THE PLANS. CONSTRUCT CURB & GUTTER.
4. CLOSE FRONTAGE ROAD.
5. REMOVE EXISTING PAVING. RELOCATE EXISTING FIRE HYDRANT. CONSTRUCT CURB & GUTTER, BACKING CURB, ASPHALT SHARED USE PATH, CONCRETE PEDESTRIAN RAMPS, AND DETECTABLE WARNING SURFACES. PERFORM FULL DEPTH PAVING AND CONSTRUCT CURB FOR PROPOSED TURNAROUND.
6. INSTALL PERMANENT SIGNING & PAVEMENT MARKINGS AS SHOWN ON THE PLANS.

1. REMOVE PHASE 2 TEMPORARY TRAFFIC CONTROL DEVICES AND SIGNS THAT ARE NO LONGER NEEDED.
2. CLOSE FRONTAGE ROAD. SET TEMPORARY TRAFFIC CONTROL DEVICES ALONG SOUTHBOUND MD 650 FOLLOWING MD 104.04-06.
3. INSTALL INLETS I-3 AND I-4. REMOVE EXISTING FRONTAGE ROAD MEDIAN AND RECONSTRUCT. PERFORM FULL DEPTH PATCHING. CONSTRUCT CURB & GUTTER FOR CURB LINE BUMPOUT, CONCRETE SIDEWALK AND PEDESTRIAN RAMP, AND DETECTABLE WARNING SURFACE. INSTALL PERMANENT SIGNING & PAVEMENT MARKINGS AS SHOWN ON THE PLANS.
4. REMOVE EXISTING RETAINING WALL ALONG FRONTAGE ROAD. INSTALL 1' WIDE CAST-IN-PLACE RETAINING WALL AS INDICATED ON THE PLANS. PERFORM FULL DEPTH PATCHING. INSTALL PERMANENT SIGNING & PAVEMENT MARKINGS AS SHOWN ON THE PLANS.
5. REMOVE TEMPORARY TRAFFIC CONTROL DEVICES AND SIGNS THAT ARE NO LONGER NEEDED.
6. SET TEMPORARY TRAFFIC CONTROL DEVICES ALONG SOUTHBOUND MD 650 FOLLOWING MD 104.03-10.
7. RECONSTRUCT MEDIAN ALONG MD 650 AS INDICATED ON THE PLANS. PERFORM FULL DEPTH PATCHING AND CONSTRUCT CURB AND GUTTER. REMOVE EXISTING MAST ARM POLE AND ASSOCIATED EQUIPMENT. INSTALL MAST ARM POLE WITH MAST ARMS, LED SIGNAL HEADS, AND SIGNS AS SHOWN ON THE PLANS.
8. INSTALL PERMANENT SIGNING & PAVEMENT MARKINGS AS SHOWN ON THE PLANS.

PHASE 3B – NEW HAMPSHIRE AVE. SHOPPING CENTER TO ETHAN ALLEN AVE. (MD 650 STA. 115+40 TO 120+50):

1. REMOVE PHASE 3A TEMPORARY TRAFFIC CONTROL DEVICES AND SIGNS THAT ARE NO LONGER NEEDED.
2. SET TEMPORARY TRAFFIC CONTROL DEVICES ALONG SOUTHBOUND MD 650 FOLLOWING MD 104.03-12.
3. REMOVE EXISTING MAST ARM POLE AND ASSOCIATED EQUIPMENT. INSTALL PEDESTAL POLE WITH LED SIGNAL HEADS AND APSPCS. REMOVE EXISTING BUS STOP BENCH. COORDINATE RELOCATION OF BUS STOP SIGN WITH WMATA AND MONTGOMERY COUNTY RIDE-ON. COORDINATE REMOVAL OF THE EXISTING LIGHT POLE WITHIN THE PUBLIC RIGHT-OF-WAY WITH THE NEW HAMPSHIRE AVENUE SHOPPING CENTER. RELOCATE FIRE HYDRANT. CONSTRUCT CURB & GUTTER, BACKING CURB, ASPHALT SHARED USE PATH, TYPE 2 PAVERS, CONCRETE SIDEWALK AND PEDESTRIAN RAMPS, DETECTABLE WARNING SURFACES, AND CONCRETE DRIVEWAYS.
4. RELOCATE EXISTING PUSH BUTTON IN THE CHANNELIZATION TRAFFIC ISLAND. RECONSTRUCT CURB & GUTTER, CONCRETE SIDEWALK AND PEDESTRIAN RAMPS, AND DETECTABLE WARNING SURFACES FOR THE ISLAND.

PUSH BUTTON SIGNALS SHALL BE REACTIVATED PRIOR TO ADVANCING TO THE NEXT PHASE OF WORK.
5. REMOVE TEMPORARY TRAFFIC CONTROL DEVICES AND SIGNS THAT ARE NO LONGER NEEDED.
6. SET TEMPORARY TRAFFIC CONTROL DEVICES ALONG THE SOUTHBOUND MD 650 MEDIAN FOLLOWING MD 104.03-10 AND MD 104.04-04.
7. RECONSTRUCT MEDIAN ALONG MD 650 AS INDICATED ON THE PLANS. PERFORM FULL DEPTH PATCHING.
8. REMOVE TEMPORARY TRAFFIC CONTROL DEVICES AND SIGNS THAT ARE NO LONGER NEEDED.
9. INSTALL PERMANENT SIGNING & PAVEMENT MARKINGS AS SHOWN ON THE PLANS.

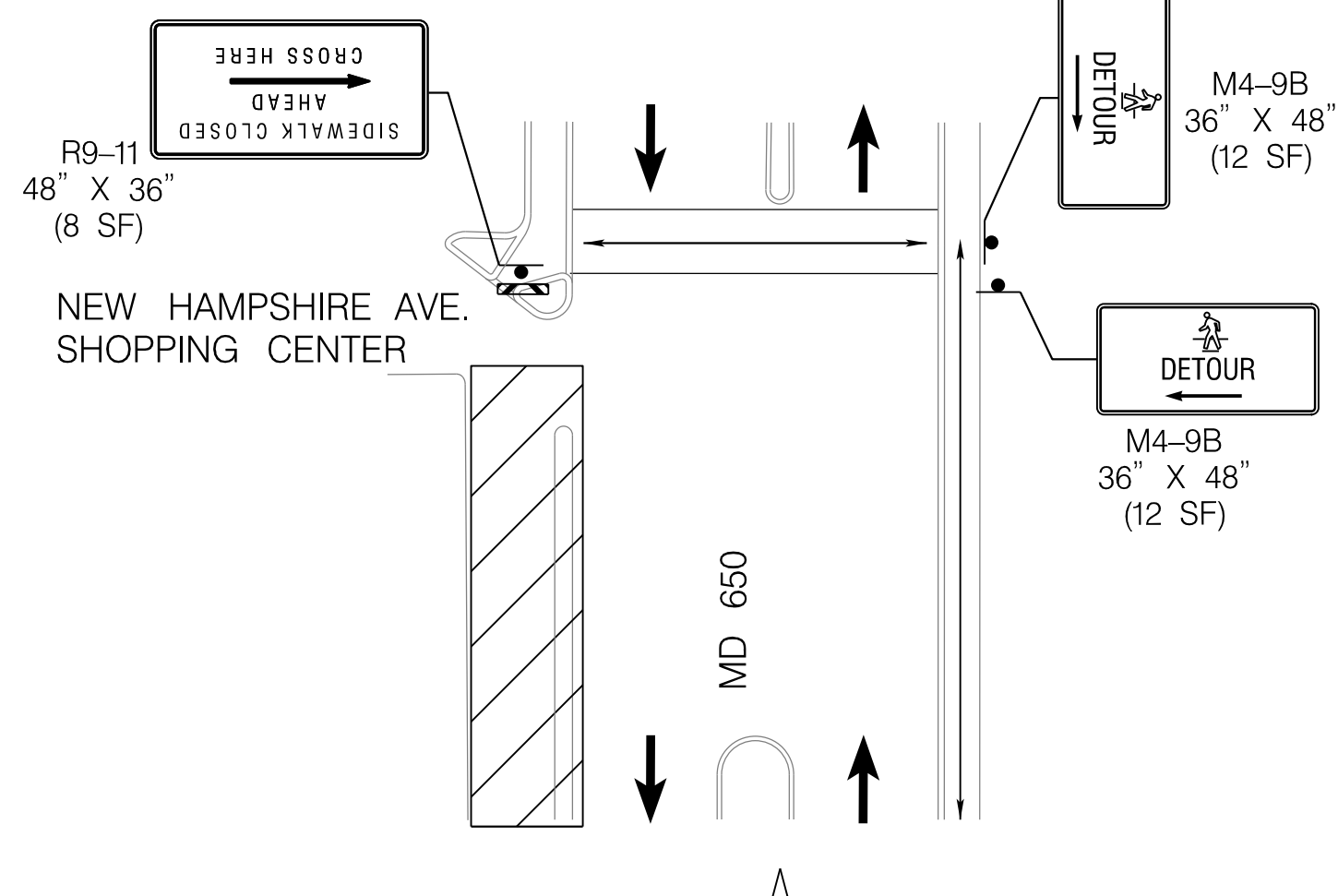
1. REMOVE PHASE 3B TEMPORARY TRAFFIC CONTROL DEVICES AND SIGNS THAT ARE NO LONGER NEEDED.
2. SET TEMPORARY TRAFFIC CONTROL DEVICES ALONG WESTBOUND ETHAN ALLEN AVENUE FOLLOWING MD 104.03-12. SET TEMPORARY TRAFFIC CONTROL DEVICES ALONG SOUTHBOUND MD 650 FOLLOWING MD 104.04-06.
3. RELOCATE EXISTING PEDESTAL POLE AND APS/CPs TO NEW FOUNDATION. INSTALL INLET I-5 AND MANHOLE MH-1. PERFORM CURB LINE BUMPOUT, CONSTRUCT ASPHALT SHARED USE PATH, TYPE 2 PAVERS, CONCRETE SIDEWALK AND PEDESTRIAN RAMPS, DETECTABLE WARNING SURFACES, AND CONCRETE DRIVEWAY.

PUSH BUTTON SIGNALS SHALL BE REACTIVATED PRIOR TO ADVANCING TO THE NEXT PHASE OF WORK.
4. REMOVE TEMPORARY TRAFFIC CONTROL DEVICES AND SIGNS THAT ARE NO LONGER NEEDED.
5. INSTALL PERMANENT SIGNING & PAVEMENT MARKINGS AS SHOWN ON THE PLANS.

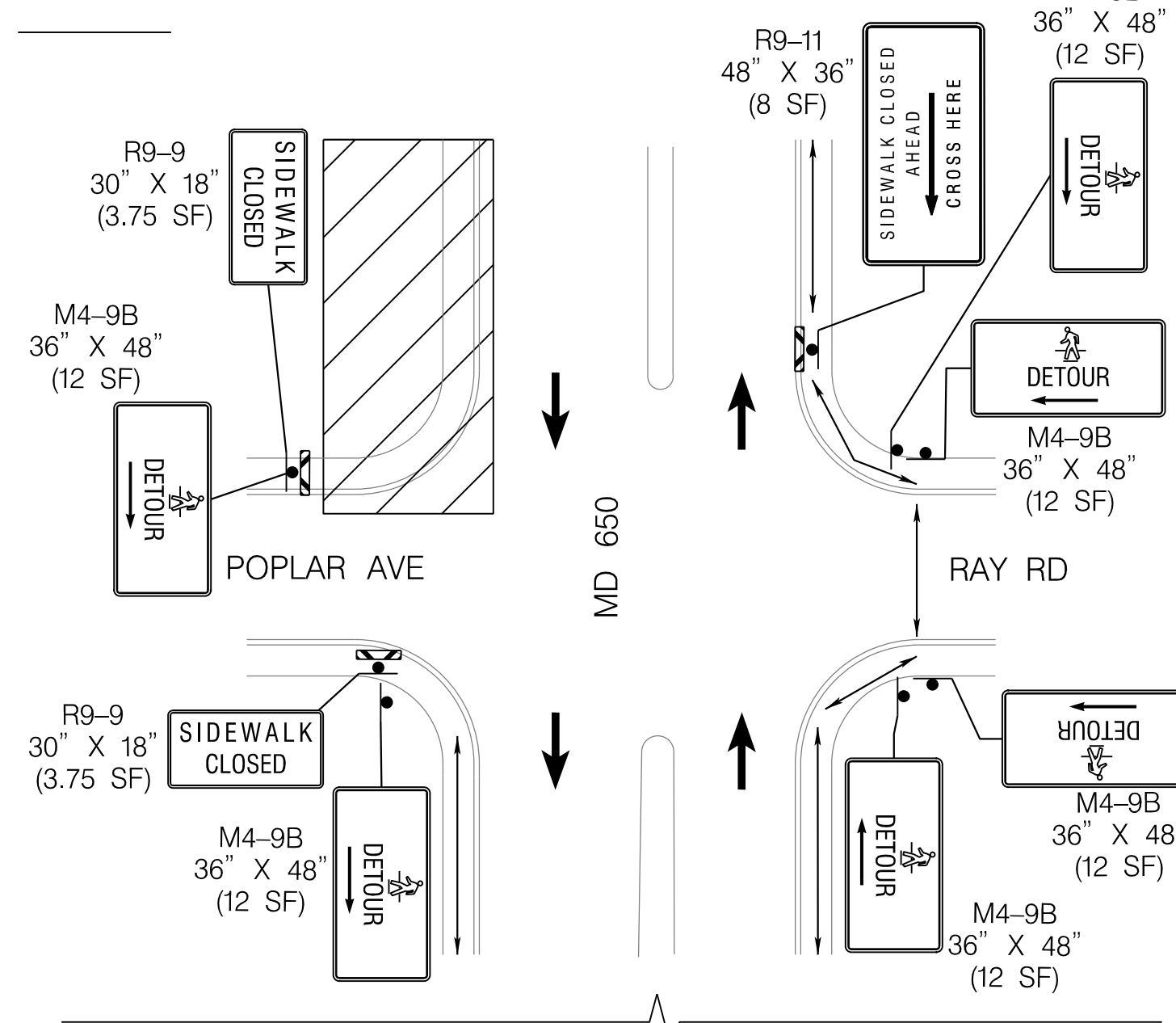
REVISIONS	MAINTENANCE OF TRAFFIC NARRATIVE										
60% PLANS NOVEMBER 2021	SCALE _____ NTS _____ DATE _NOVEMBER 2021_ CONTRACT NO. _T.B.D._										
	DESIGNED BY _____ SAB _____					COUNTY _MONTGOMERY					
	DRAWN BY _____ SAB _____					LOGMILE MD 650 0.040-0.830					
	CHECKED BY _____ R.J.G _____										
	F.A.P. NO. _____ T.B.D. _____										
	DRAWING NO. MT01				01	OF	02	SHEET NO. 31			OF 44

POPLAR AVE TO THE NEW HAMPSHIRE AVE
SHOPPING CENTER (STA. 115+50)

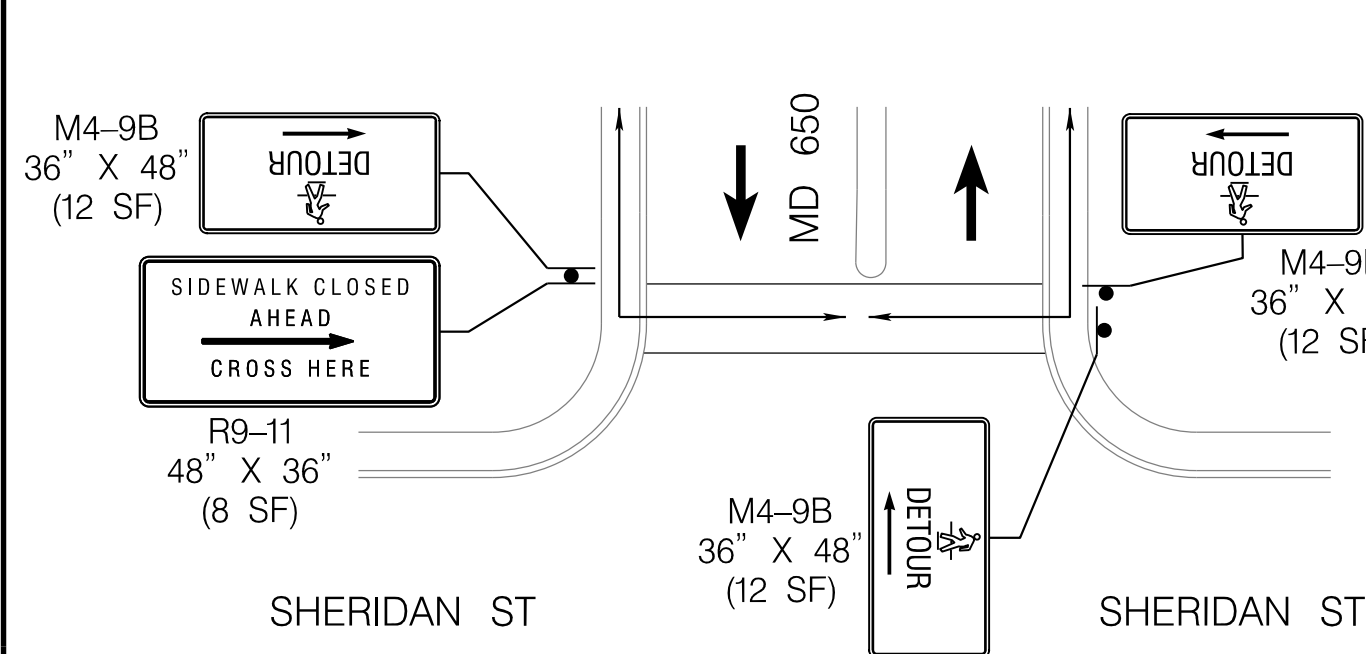
SECTION 1



SECTION 2

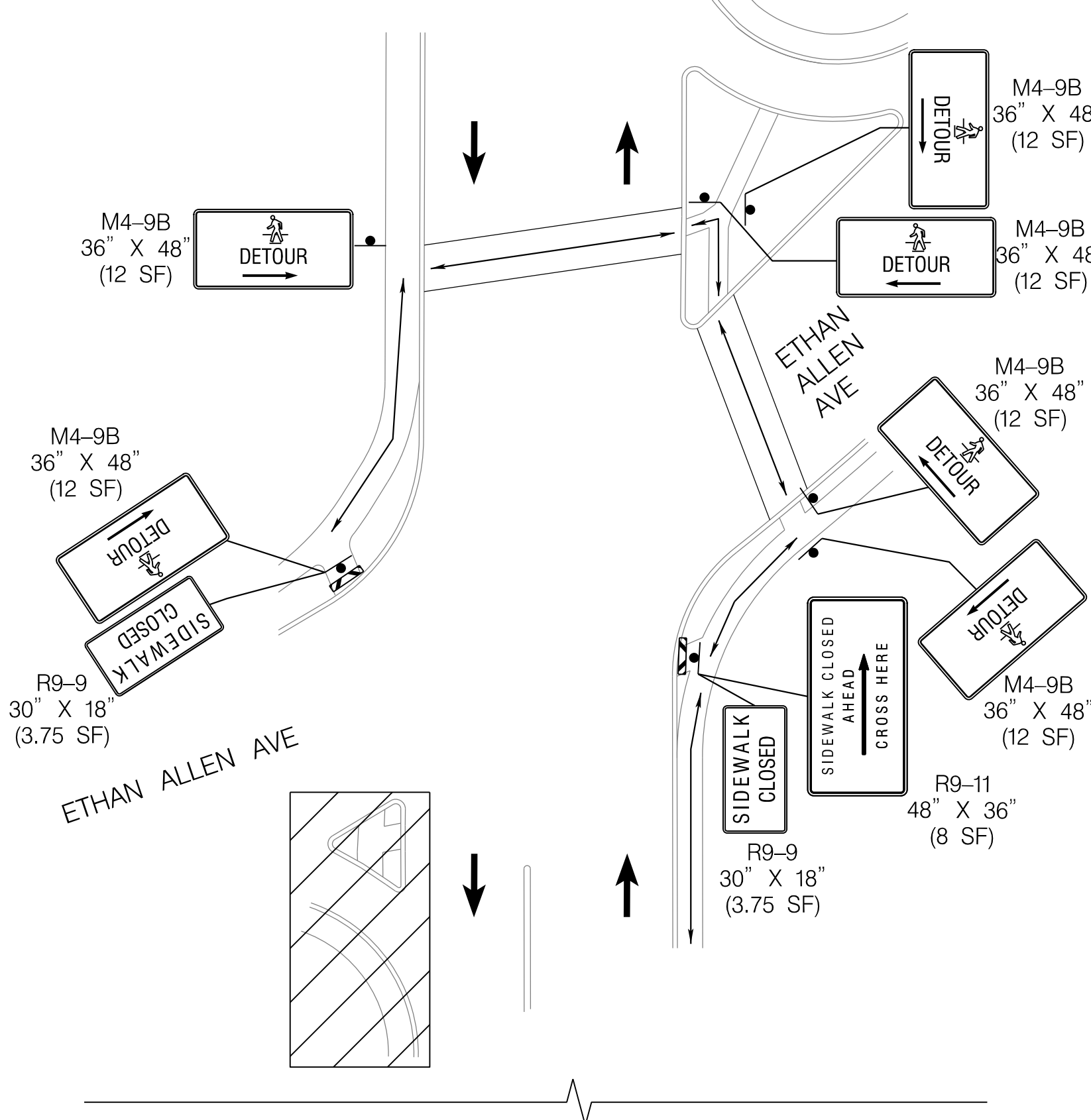


SECTION 3

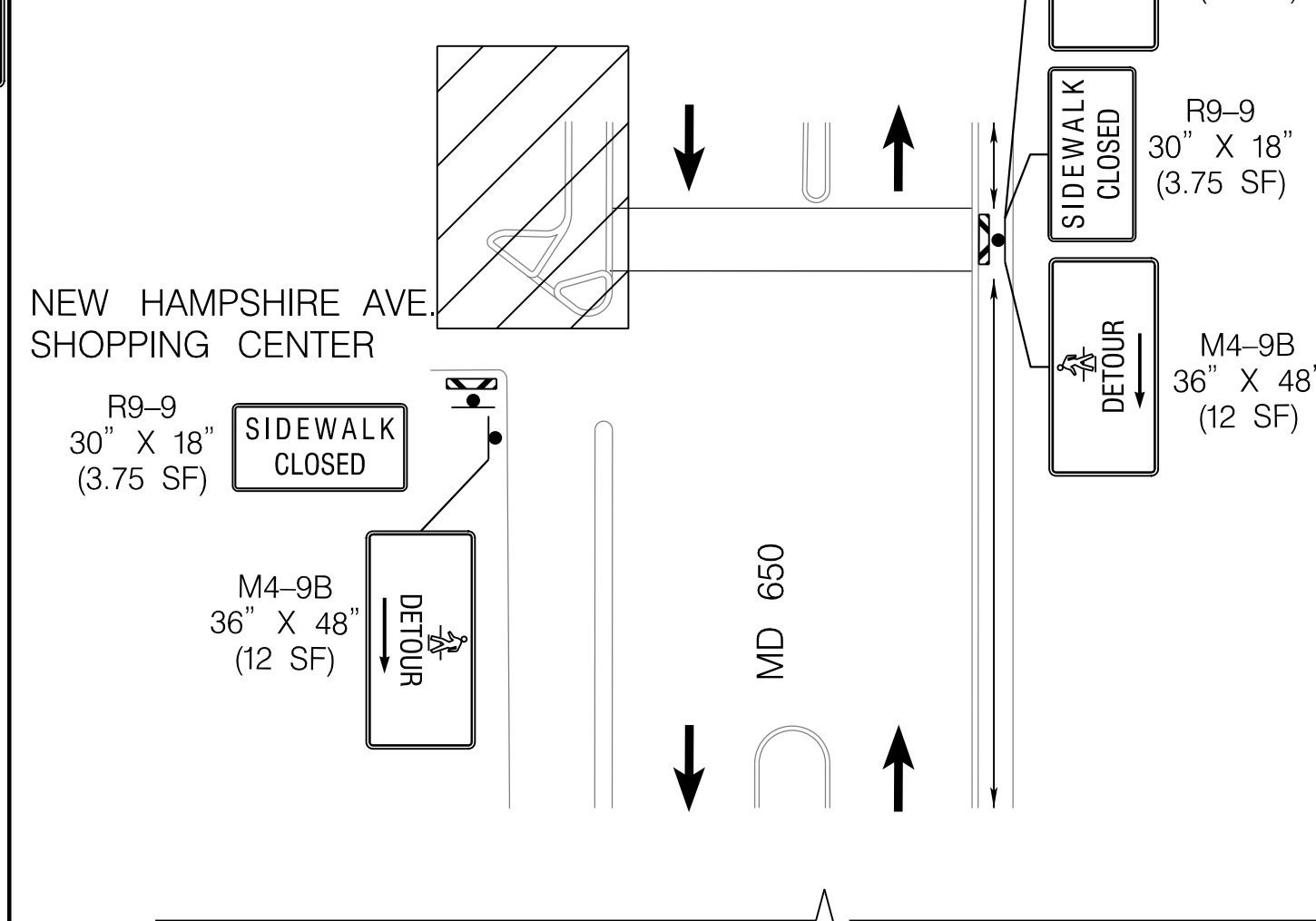


NEW HAMPSHIRE AVE. SHOPPING CENTER
(STA. 115+50) TO ETHAN ALLEN AVE.

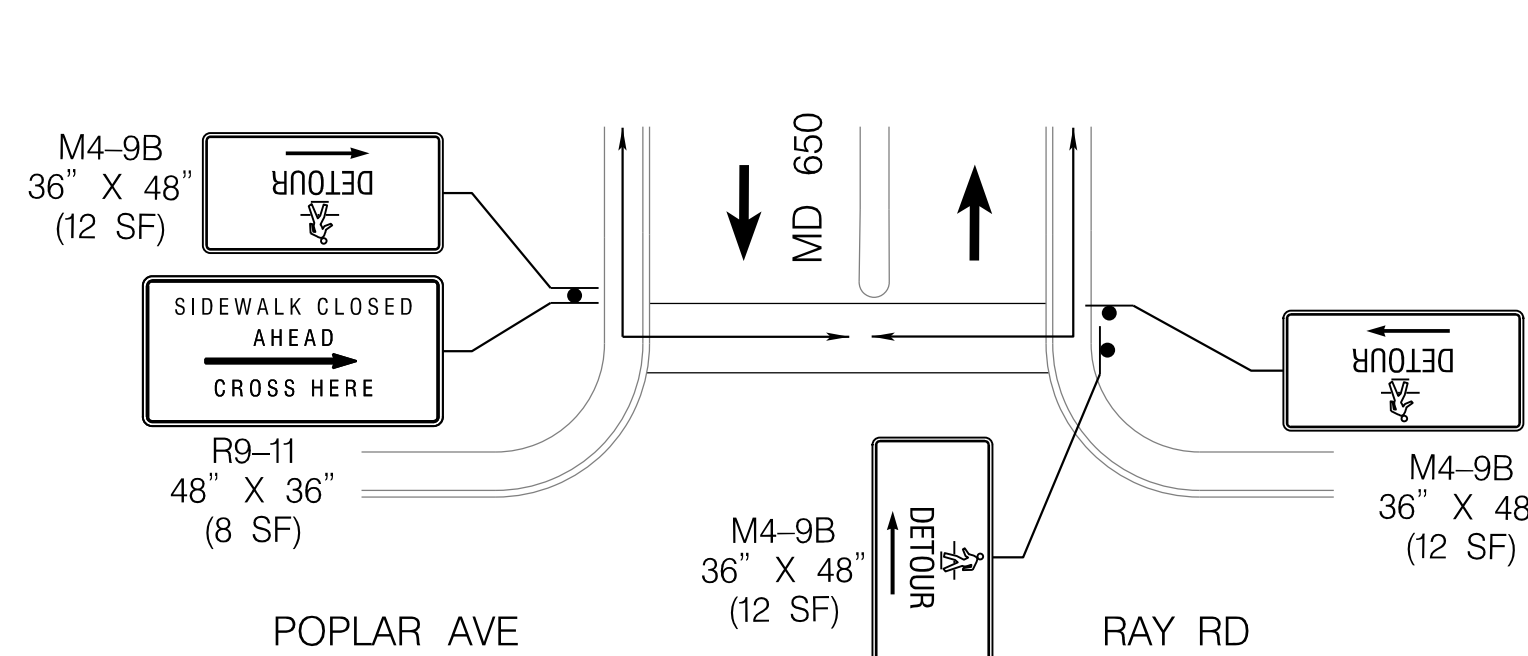
SECTION 1



SECTION 2

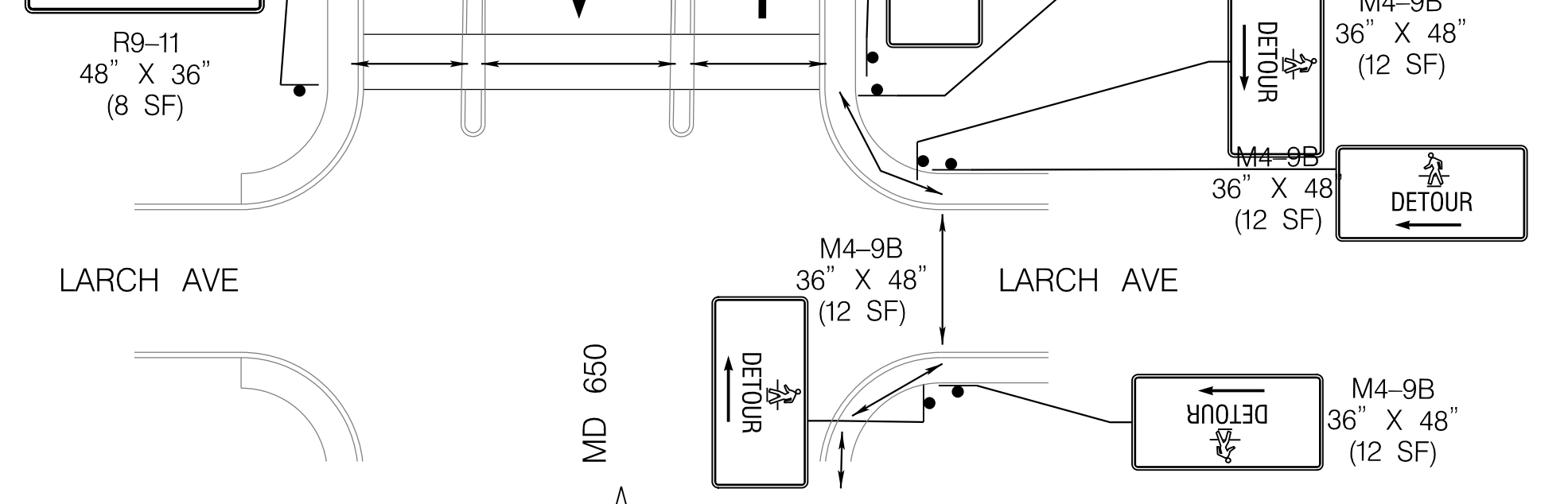


SECTION 3

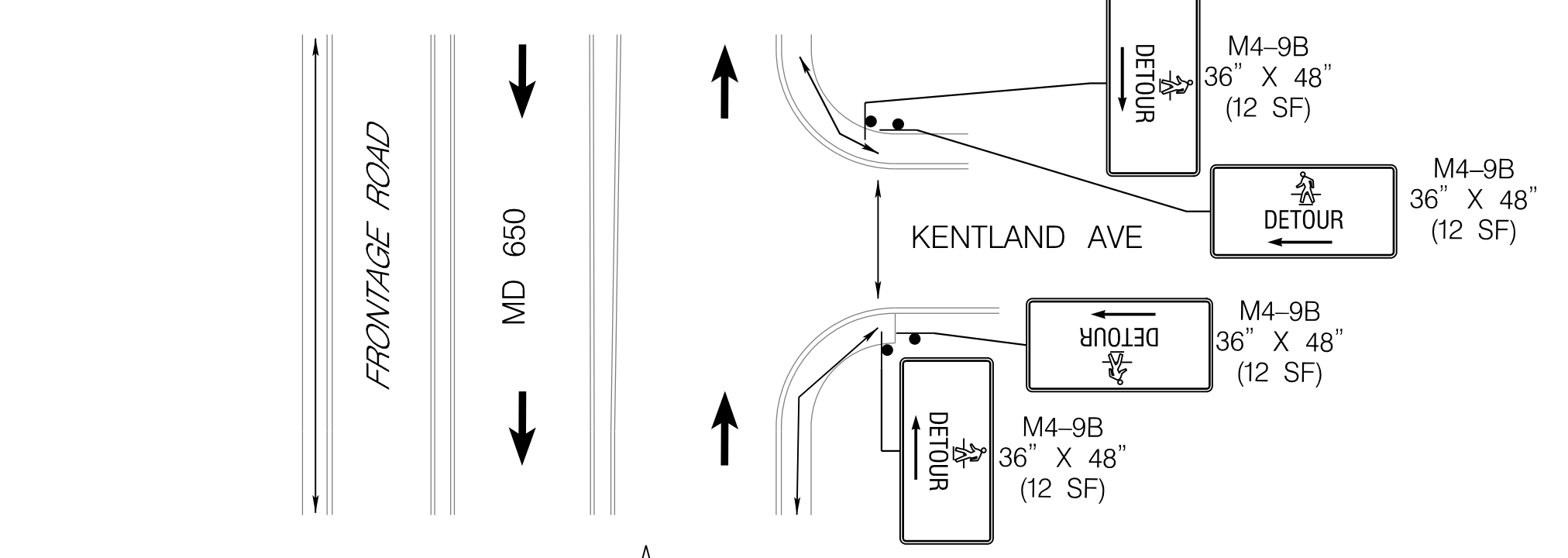


ETHAN ALLEN AVE.
TO AUBURN AVE.

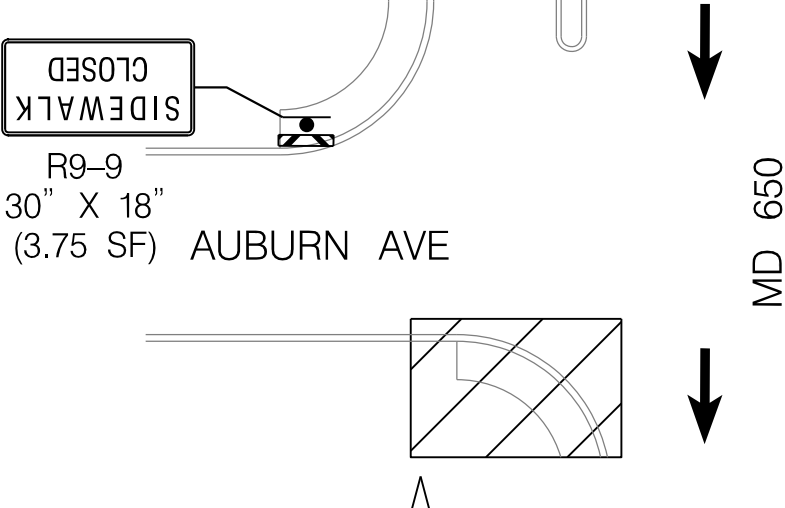
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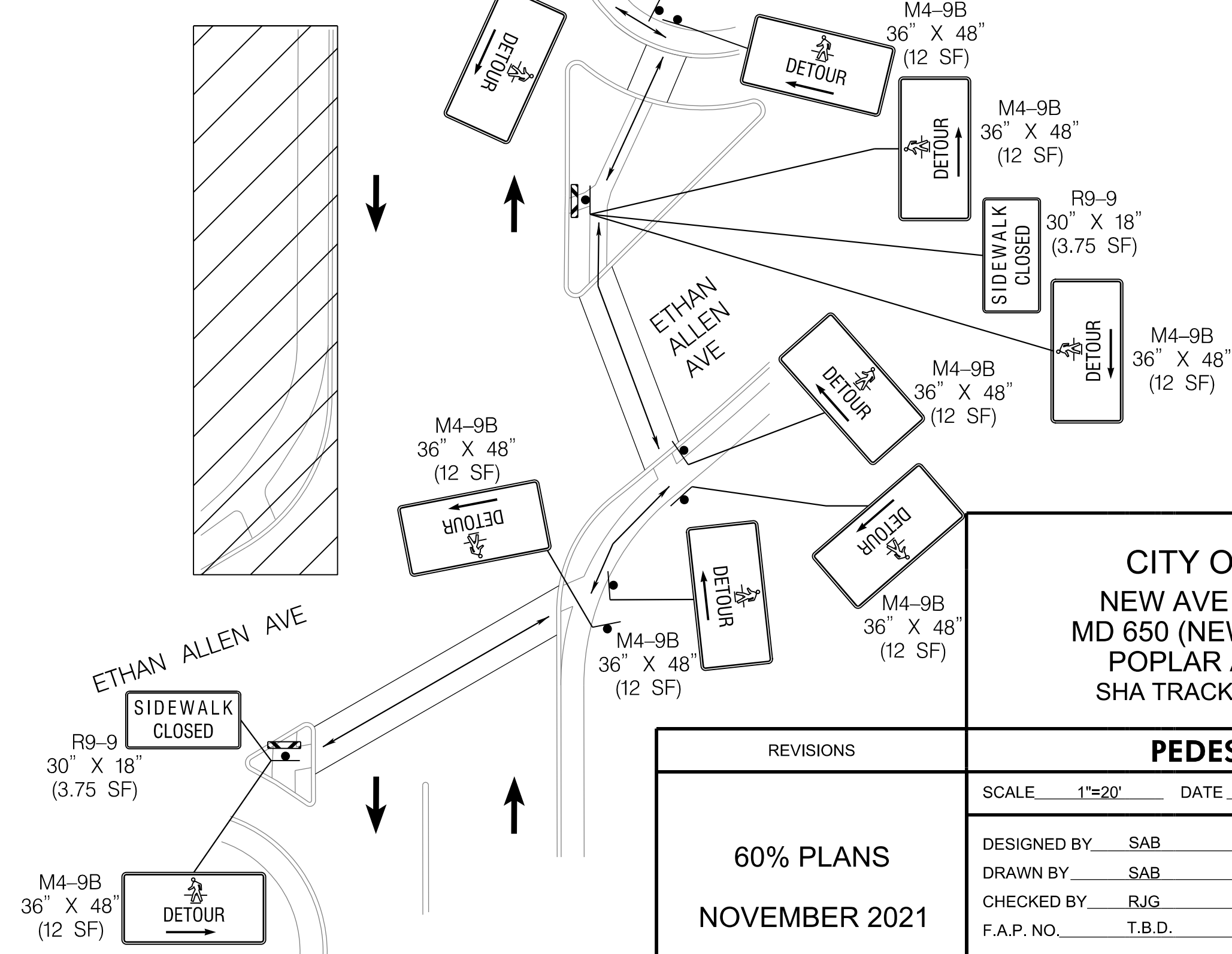
SECTION 2



SECTION 3



SECTION 4



CITY OF TAKOMA PARK
NEW AVE BIKEWAY, SECTION B
MD 650 (NEW HAMPSHIRE AVENUE)
POPLAR AVE TO AUBURN AVE
SHA TRACKING NO. 20-AP-MO-020-XX

REVISIONS		PEDESTRIAN DETOUR	
60% PLANS NOVEMBER 2021		SCALE 1"=20'	DATE NOVEMBER 2021 CONTRACT NO. T.B.D.
		DESIGNED BY SAB	COUNTY MONTGOMERY
		DRAWN BY SAB	LOGMILE MD 650 0.040-0.830
		CHECKED BY RJG	F.A.P. NO. T.B.D.
		DRAWING NO. MT02 02 OF 02	SHEET NO. 31A OF 44

PLOTTED: 11/2/2021
FILE: \\ad.rkk.com\its\Cloud\Projects\2020\20007_NewAveSecB\CADD\Plans\MT-0002_NewAveBike_B.dgn

BY: kboampong -



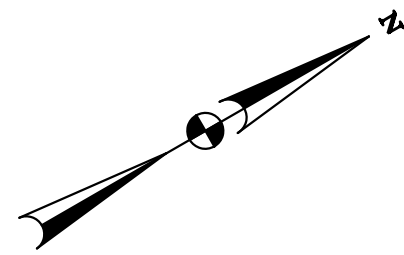
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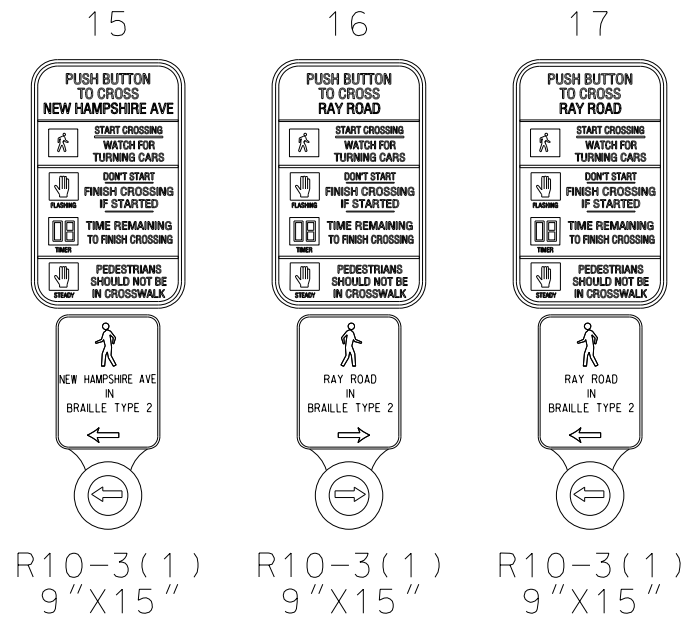
LEGEND

- SIGN
- WORK AREA
- DIRECTION OF TRAFFIC
- DETECTABLE BARRICADE
- PEDESTRIAN MOT



MD 650 IS ASSUMED TO RUN
IN A NORTH-SOUTH DIRECTION

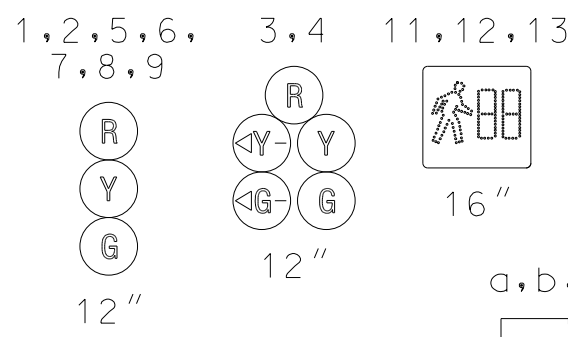
EXISTING SIGNS
TO REMAIN



EXISTING SIGNS
TO BE RELOCATED

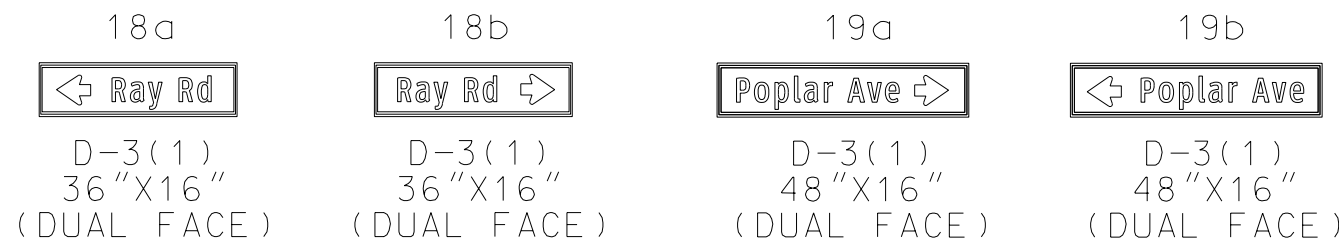


EXISTING LED SIGNALS
TO REMAIN

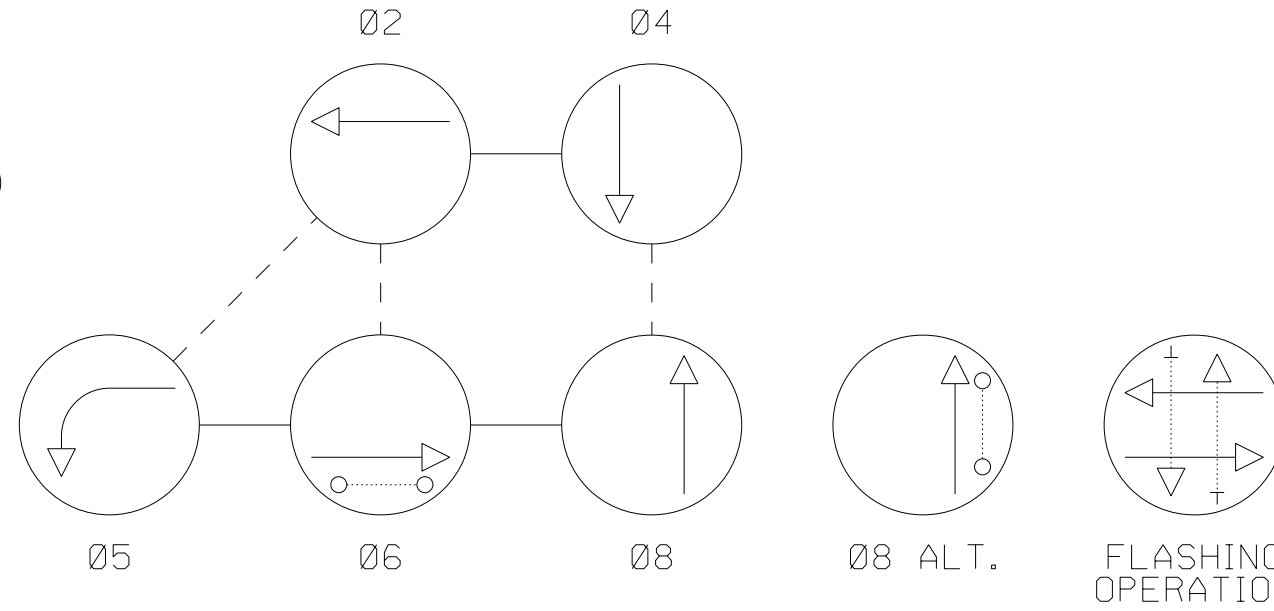


EXISTING IP-BASED VIDEO
DETECTION TO REMAIN

EXISTING SIGNS TO REMAIN



NEMA PHASING



NOTE:
PHASES ASSOCIATED BY A DASHED LINE WILL OPERATE CONCURRENTLY.
PHASES ASSOCIATED BY A SOLID LINE WILL NOT OPERATE CONCURRENTLY.

TO WASHINGTON DC

MD 650 (NEW HAMPSHIRE AVE) SB

MD 650 (NEW HAMPSHIRE AVE) NB

TO MD 193

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APPROVALS	
TEAM LEADER	
ASST. DIV. CHIEF	
DIVISION CHIEF	
OFFICE DIRECTOR	

REVISIONS	
C	RELOCATE APS/CPS FOR SHARED USE PATH SHA NO. 20-AP-MD-020-xx, TMS NO. 11-2021
B	INSTALL E/P LEFT TURN FOR SB APS/CPS FOR EAST & NORTH LEOS SHA NO. XX3545168, TMS NO. K-B20 03-2011
A	ADDITIONAL LOOP DETECTOR TO BE ADDED TO WB RAY ROAD P-343-001-385 04-1984

OFFICE OF TRAFFIC & SAFETY
TRAFFIC ENGINEERING DESIGN DIVISION

MDOT
MARYLAND DEPARTMENT
OF TRANSPORTATION
STATE HIGHWAY
ADMINISTRATION

NEW AVE BIKEWAY
SECTION B
MD 650 (NEW HAMPSHIRE AVE)
AT RAY RD/POPLAR AVE

TRAFFIC SIGNAL PLAN

SCALE 1" = 20' DATE JUNE 1984 CONTRACT NO. P-343-001-385

DESIGNED BY DCD COUNTY PRINCE GEORGE'S
DRAWN BY MATTHAI LOGMILE 16065000.43
CHECKED BY S.RENZI TMS NO.
MDE/PRD SEE TITLE SHEET TOD NO.

TS NO. 713C DRAWING SG-1 OF 3 SHEET NO. 32 OF 42

PLOTTED: 10/28/2021
FILE: \\ad.rk.com\ys\Cloud\Projects\2020\2007_NewAveSecB\CADD\Plans\psg-0001_NewAveBike_Ray_Poplar.dgn

MD 650 IS ASSUMED TO RUN
IN A NORTH-SOUTH DIRECTION

PROPOSED ACCESSIBLE PUSHBUTTONS AND SIGNS



NEW HAMPSHIRE AVE
R10-3(1)
9"X15"



NEW HAMPSHIRE AVE
R10-3(1)
9"X15"

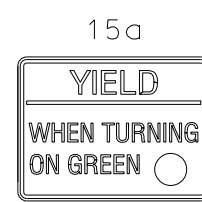
EXISTING SIGNS TO BE REMOVED



30"X36"

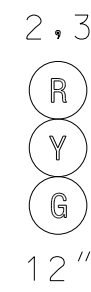


9"X12"



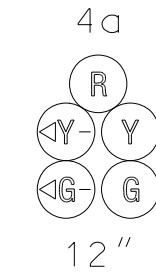
R1-2c
36"X48"

EXISTING LED SIGNALS TO REMAIN

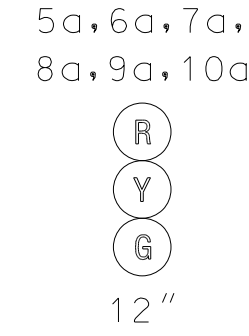


12"

EXISTING LED SIGNALS TO BE REMOVED



12"



16"

PROPOSED SIGNS

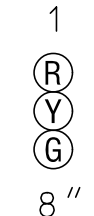


R3-5L
30"X36"

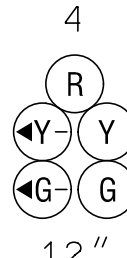


R4-7
24"X30"

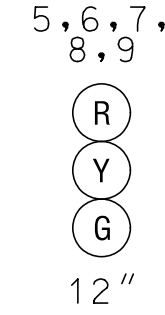
PROPOSED LED SIGNALS



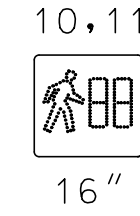
8"



12"

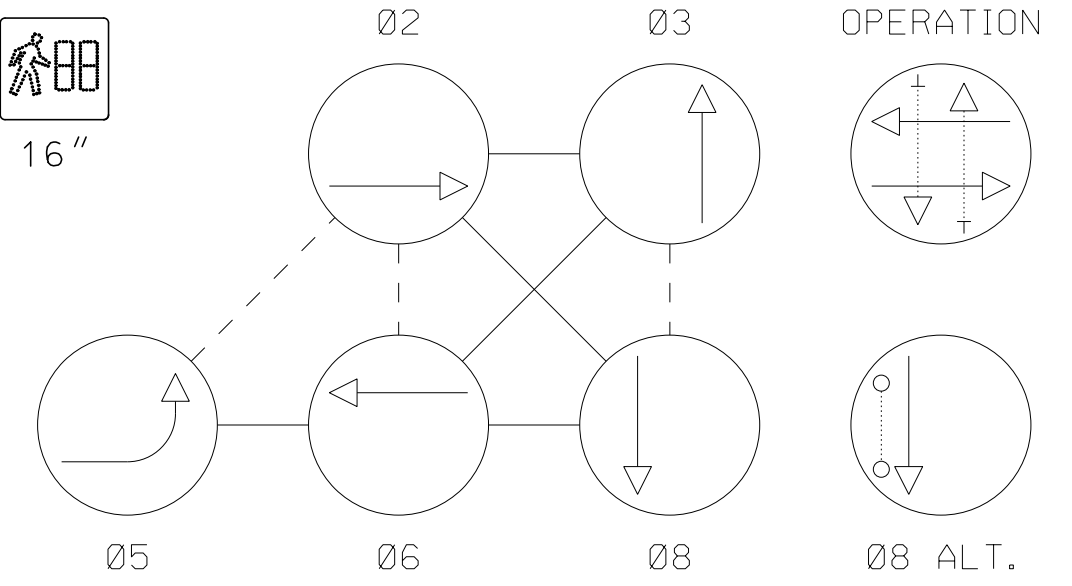


12"

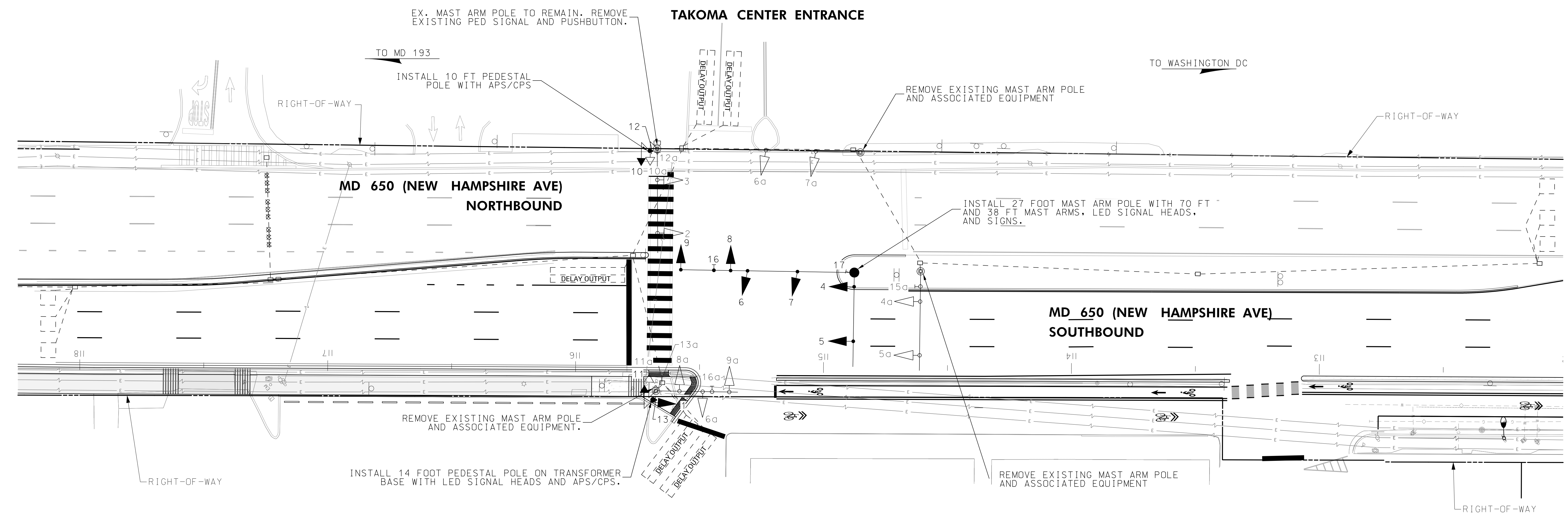


16"

NEMA PHASING



NOTE:
PHASES ASSOCIATED BY A DASHED LINE WILL OPERATE CONCURRENTLY.
PHASES ASSOCIATED BY A SOLID LINE WILL NOT OPERATE CONCURRENTLY.



60% DESIGN REVIEW NOTES

1. APS AND ADA DESIGN WILL CONTINUE COORDINATION IN SUBSEQUENT SUBMISSIONS. APS DESIGN WAIVER MAY BE REQUIRED FOR FINAL APS ORIENTATION.
2. PROPERTY ACQUISITION MAY BE REQUIRED IF SIGNAL EQUIPMENT MUST BE INSTALLED OUTSIDE OF ROW

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TEAM LEADER	
ASST. DIR. CHIEF	
DIVISION CHIEF	
OFFICE DIRECTOR	

REVISIONS	
C	PARTIAL SIGNAL RECONSTRUCTION 11-2021 SHA NO. 20-AP-MD-020-xx, T1MS NO.
B	RELOCATE SB MAINLINE 08-10-1986 CONDUIT TO MEDIAN, INSTALL SAWCUT LOOP WIRE SHA NO. P-839-501-385
A	SIGNAL RECONSTRUCTION 01-07-1986 WITH MAST ARMS SHA NO. P-839-501-385

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MARYLAND DEPARTMENT
OF TRANSPORTATION
STATE HIGHWAY
ADMINISTRATION

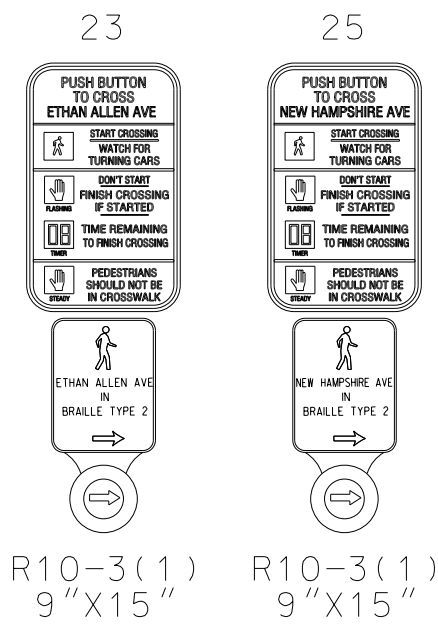
NEW AVE BIKEWAY
SECTION B
MD 650 (NEW HAMPSHIRE AVE)
AT TAKOMA CENTER ENTRANCE

TRAFFIC SIGNAL PLAN			
SCALE	1" = 20'	DATE	08-30-1972
CONTRACT NO.			
DESIGNED BY		COUNTY	PRINCE GEORGE'S
DRAWN BY	MATTHAI	LOGMILE	16065000.70
CHECKED BY		TMS NO.	
MDE/PRD	SEE TITLE SHEET	TOD NO.	
TS NO.	698C	DRAWING	SG-2
OF	3	SHEET NO.	33
OF		OF	42

PLOTTED: 10/28/2021
FILE: \\ad.rnk.com\ys\Cloud\Projects\2020\2007_NewAveSecB\CADD\Plans\pSG-0002_NewAveBike_B_Co-Op.dgn

MD 650 IS ASSUMED TO RUN
IN A NORTH-SOUTH DIRECTION

EXISTING SIGNS
TO BE RELOCATED

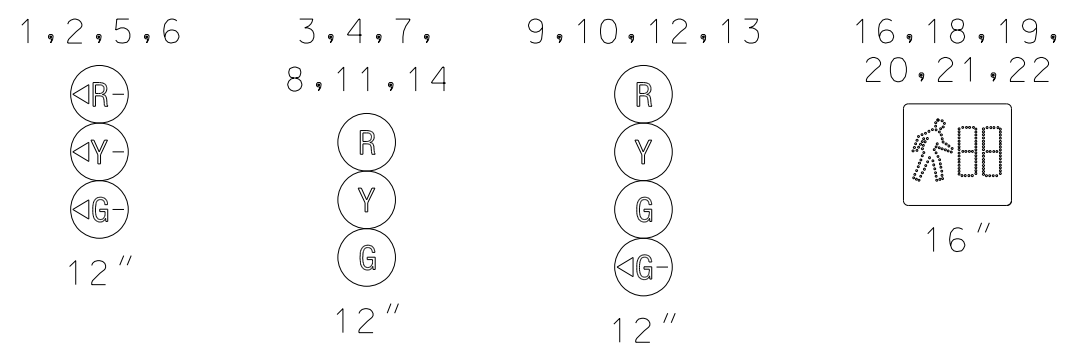


TO WASHINGTON DC

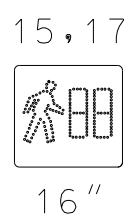
RIGHT-OF-WAY

RIGHT-OF-WAY

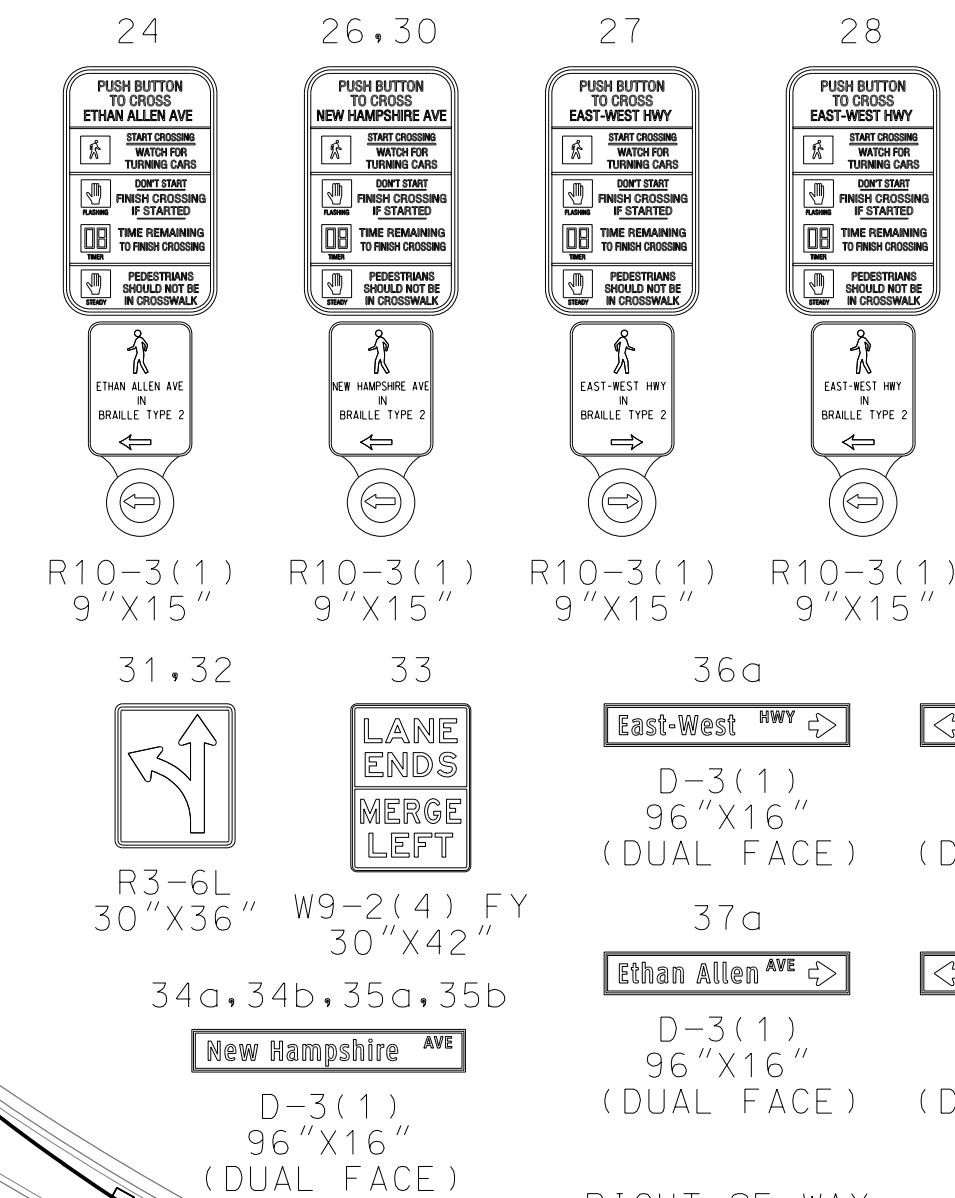
EXISTING LED SIGNALS
TO REMAIN



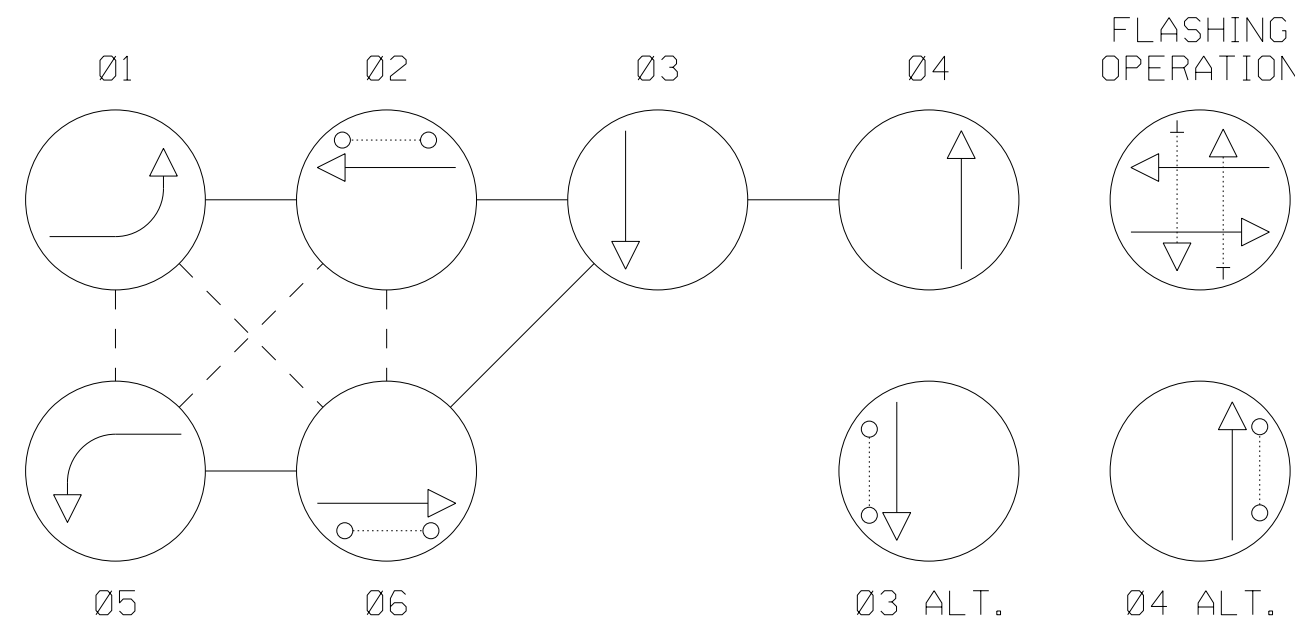
EXISTING LED SIGNALS
TO BE RELOCATED



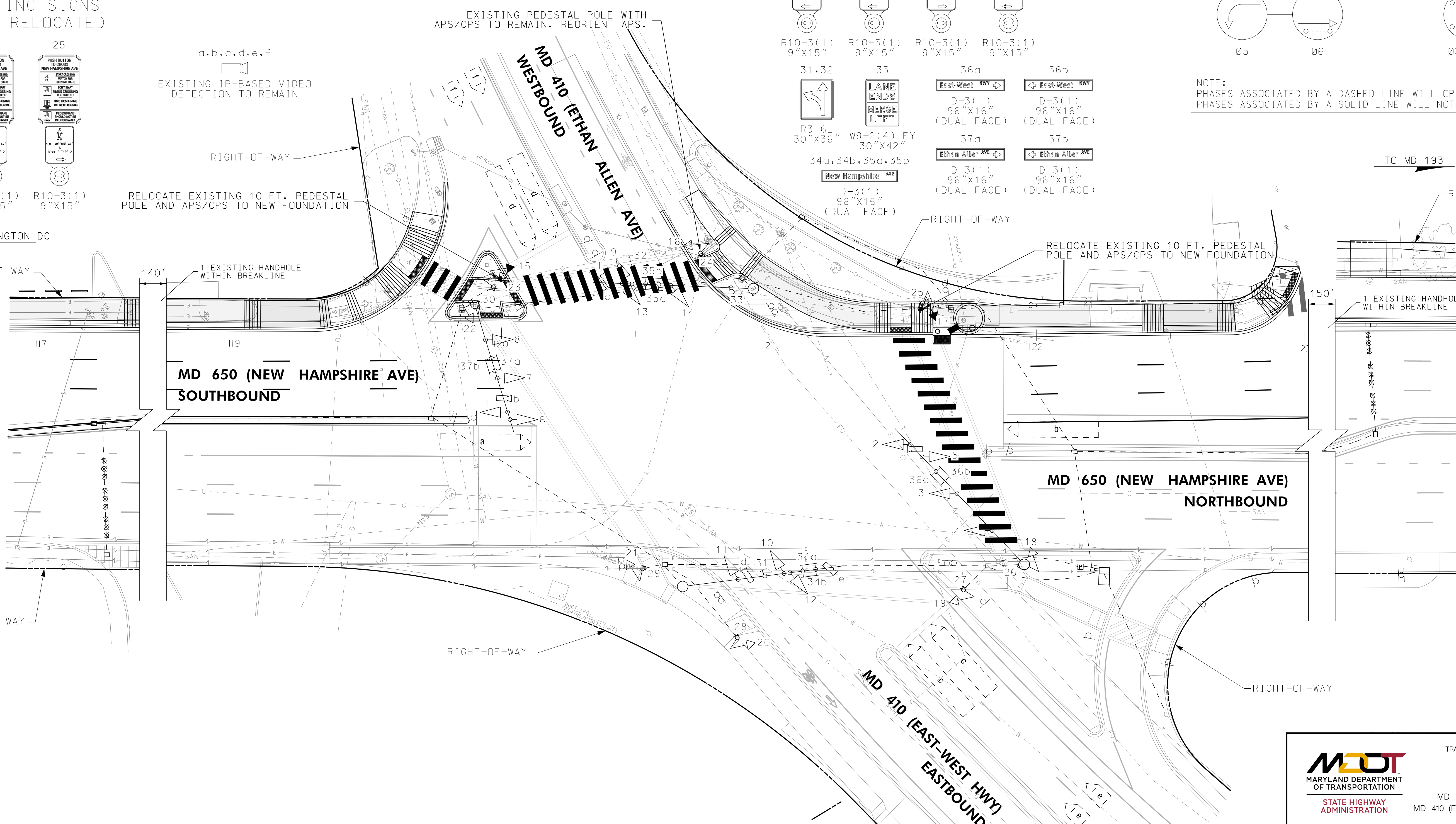
EXISTING SIGNS TO REMAIN



NEMA PHASING



NOTE:
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PHASES ASSOCIATED BY A SOLID LINE WILL NOT OPERATE CONCURRENTLY.



OFFICE OF TRAFFIC & SAFETY
TRAFFIC ENGINEERING DESIGN DIVISION

NEW AVE BIKEWAY
SECTION B
MD 650 (NEW HAMPSHIRE AVE) AT
MD 410 (EAST-WEST HWY /ETHAN ALLEN AVE)

TRAFFIC SIGNAL PLAN

SCALE 1" = 20' DATE 08-21-1972 CONTRACT NO. P-343-001-385

DESIGNED BY COUNTY PRINCE GEORGE'S
DRAWN BY MATTHAI LOGMILE 16065000.81
CHECKED BY TIMS NO.
MDE/PRD SEE TITLE SHEET TOD NO.

TS NO. 319F DRAWING SG-3 OF 3 SHEET NO. 34 OF 42



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APPROVALS	REVISIONS
TEAM LEADER	RELOCATE APS/CPS FOR SHARED USE PATH 11-2021 SHA NO. 20-AP-MD-020-xx, TIMS NO.
ASST. DIV. CHIEF	SIGNAL HEAD & APS/CPS 11-2015 MODIFICATIONS DUE TO GEOMETRICS 11-2015 TIMS NO. M794 SHA NO. MD0705125
DIVISION CHIEF	SIGNAL RECONSTRUCTION 10-2009 SHA NO. PG7425177 TIMS NO. J938
OFFICE DIRECTOR	STV SDY WS JWB RDB CDS TH

PLOTTED: 10/28/2021
FILE: \\ad.rnk.com\ys\Cloud\Projects\2020\2007_NewAveSecB\CADD\Plans\pSG-0003_NewAveBike_B_EthanAllen.dgn

CRITERIA

THE CONTRACTOR SHALL BE GOVERNED BY THE STANDARDS AND REQUIREMENTS OF THE FOLLOWING PUBLICATIONS, EXCEPT AS MODIFIED BY THE SPECIAL PROVISIONS OF THIS CONTRACT:

DESIGN

MDOT SHA - "MARYLAND MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", 2011 EDITION AND SUBSEQUENT REVISIONS. (MDMUTCD)

A A S H T O - "HIGHWAY SAFETY DESIGN AND OPERATIONS GUIDE"-1997

A A S H T O - "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS LUMINAIRES AND TRAFFIC SIGNALS", 2001 EDITION (CATEGORY II FOR ALL OVERHEAD AND CANTILEVER SIGN STRUCTURES).

MATERIALS AND CONSTRUCTION

MDOT SHA - "STANDARD SPECIFICATIONS FOR CONSTRUCTION & MATERIALS", MOST CURRENT EDITION AND SUBSEQUENT REVISIONS AND SUPPLEMENTS.

MDOT SHA - "BOOK OF STANDARDS FOR HIGHWAY AND INCIDENTAL STRUCTURES", MOST CURRENT EDITION AND SUBSEQUENT REVISIONS AND SUPPLEMENTS.

DESIGN WIND

100 MPH - WOOD SUPPORTS
10 YEAR RECURRENCE INTERVAL

100 MPH - GROUND MOUNT SIGN STEEL SUPPORTS
10 YEAR RECURRENCE INTERVAL

100 MPH - OVERHEAD AND CANTILEVER STRUCTURES
50 YEAR RECURRENCE INTERVAL

ALL DISTRICTS

DESIGN STRESS

SOIL BEARING PRESSURE - S = 3,000 P.S.F. (ASSUMED)
SEE MATERIAL & CONSTRUCTION ABOVE AND SPECIAL PROVISIONS FOR DESIGN STRESSES FOR STRUCTURAL STEEL, ALUMINUM, REINFORCING STEEL AND CONCRETE.

CHAMFER

ALL EXPOSED EDGES OF CONCRETE SHALL HAVE A 3/4" X 3/4" CHAMFER.

CLASSIFICATION OF SIGNS

SIGNS ARE DIVIDED INTO TWO (2) GENERAL CATEGORIES.

I. GUIDE SIGNS

A) STRUCTURAL TYPES

- OH - OVERHEAD
- C - CANTILEVER
- GM - GROUND MOUNT, BREAKAWAY OR NON-BREAKWAY
- BM - BRIDGE MOUNTED

MATERIAL - EXTRUDED ALUMINUM
COPY - DIRECT APPLIED

I) HIGH INTENSITY (NEW SIGNS AND REVISIONS TO EXISTING SIGNS)

2. STANDARD SIGNS (REGULATORY, WARNING, ETC.)

A) STRUCTURAL TYPES

WOOD SUPPORTS
SQUARE TUBE

MATERIAL - SHEET ALUMINUM
COPY - DIRECT APPLIED

IDENTIFICATION OF SIGNS AND PANELS

GUIDE SIGNS

EACH GUIDE SIGN IS IDENTIFIED BY A SIGN NUMBER ON THE PLANS AND IN THE TABULATIONS. (GM-1, GM-2, GM-3, etc)
SIGNS ON STRUCTURES ARE IDENTIFIED WITH A NUMBER AND WHERE VARIATIONS OCCUR, A LOWER CASE LETTER. (OH-1a, OH-1b, OH-1c)

STANDARD SIGNS

STANDARD SIGNS ARE IDENTIFIED BY PANEL NUMBERS AND ARE CLASSIFIED AS FOLLOWS

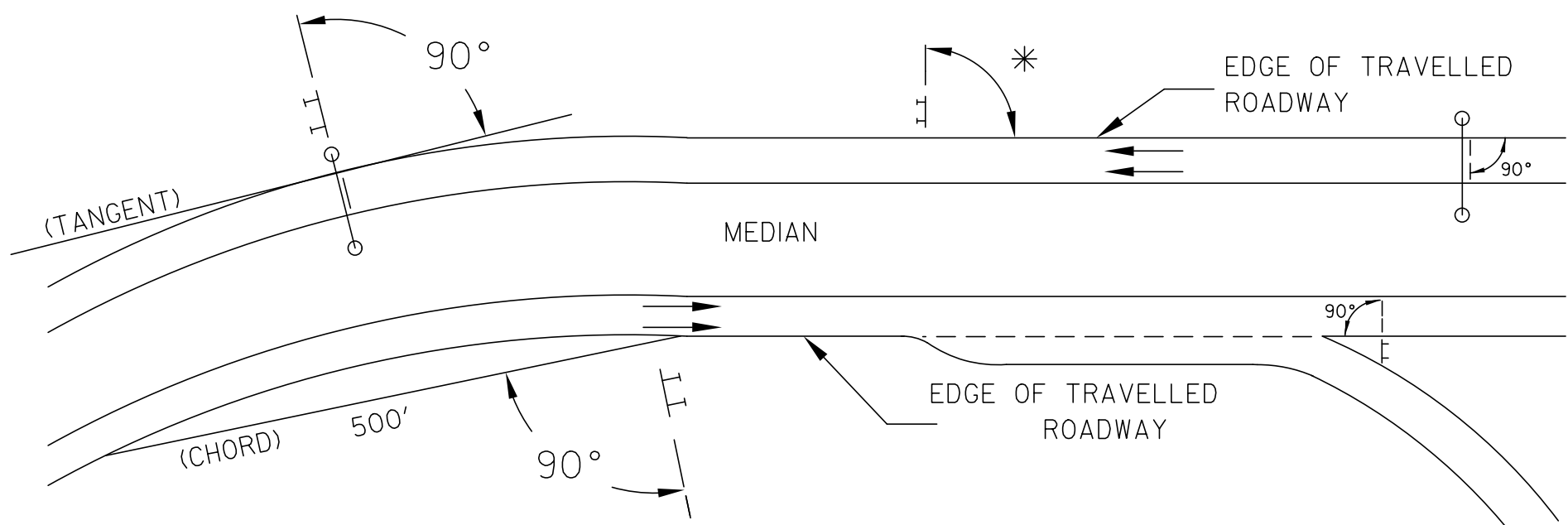
- R - REGULATORY
- W - WARNING
- M - ROUTE MARKERS AND ACCESSORIES
- D - DESTINATION AND MILEAGE PANELS
- S - SCHOOL

PANELS SHALL BE DESIGNATED TO AGREE WITH MARYLAND STANDARD SIGN BOOK. EACH STANDARD SIGN IS IDENTIFIED FIRST BY THE SHEET NUMBER, THEN BY THE NUMERICAL ORDER OF THE SIGN AS IT APPEARS ON THE PLAN.
FOR EXAMPLE SHEET SN 2.1-101,102,103, ETC. SHEET SN 2.2-201,202,203,ETC.

PANEL LAYOUT AND ALPHABETS

1. GUIDE SIGN PANEL LAYOUTS ARE BASED ON THE A.A.S.H.T.O. MANUALS NOTED ABOVE.
2. STANDARD SIGN PANEL LAYOUTS ARE BASED ON THE MDMUTCD WITH SPECIFICATIONS DETAILED IN THE MARYLAND STATE HIGHWAY ADMINISTRATION PUBLICATION, "STANDARD SIGN BOOK", AVAILABLE ONLINE AT http://apps.roads.maryland.gov/businesswithsha/bizstdsspecs/desmanualstdpub/publicationsonline/oofs/internet_signbook.asp

ORIENTATION OF SIGN FACES



* UNDER 30 FEET FROM TRAVELLED ROADWAY TO NEAR EDGE OF SIGN - 93° AWAY FROM THE ROAD TO AVOID SPECULAR REFLECTION AS INDICATED IN 813.03 OF THE MARYLAND STANDARD SPECIFICATIONS FOR CONSTRUCTION AND MATERIALS.

OVER 30 FEET FROM TRAVELLED ROADWAY TO NEAR EDGE OF SIGN - 90°

REFLECTORIZATION

BACKGROUNDS, BORDERS, TEXTS AND ALL OTHER ELEMENTS OF SIGN PANELS SHALL BE REFLECTORIZED EXCEPT WHERE NOTED. REFER TO PROJECT REQUIREMENTS FOR MORE DETAIL.

SIGN LOCATIONS

1. GUIDE SIGNS ARE LOCATED ON THE PLANS BY DIMENSION TO SURVEY STATIONS, OR WHEN NECESSARY, TO IDENTIFIABLE PHYSICAL FEATURES.
2. ALL CHANGES IN THE LOCATIONS OF SIGNS AS SHOWN ON THE PLAN SHALL HAVE THE PRIOR APPROVAL OF THE ENGINEER.

EXISTING UTILITIES

THE ENGINEER DOES NOT WARRANT OR GUARANTEE THE ACCURACY OR COMPLETENESS OF UTILITY INFORMATION SHOWN ON THE PLAN. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO LOCATE AND PROTECT ALL EXISTING FACILITIES WHICH MIGHT BE AFFECTED BY THIS WORK OR HIS OPERATION.

ROADSIDE SIGNS

1. VERTICAL ALIGNMENT
POSITION PANEL SO FACE IS PLUMB.
2. HORIZONTAL ALIGNMENT (SEE DIAGRAM ABOVE)
 - A) ON STRAIGHT ROADWAY SECTIONS, ANGLE OF SIGN FACE TO ROADWAY VARIES WITH DISTANCE FROM TRAVELLED ROADWAY TO NEAR EDGE OF SIGN - SEE DIAGRAM.
 - B) ON THE INSIDE OF HORIZONTAL CURVES, POSITION SIGN SO FACE OF PANEL MAKES AN ANGLE OF 90° WITH A CHORD BETWEEN A POINT ON NEAR EDGE OF PAVEMENT AT SIGN LOCATION AND A POINT ON EDGE OF PAVEMENT 500' IN ADVANCE OF SIGN.
 - C) ON THE OUTSIDE OF HORIZONTAL CURVES, POSITION SIGN SO FACE OF PANEL IS AT RIGHT ANGLES TO THE TANGENT OF THE CURVE AT THE SIGN LOCATION.
 - D) POSITIONING OF SIGNS AT GORES AND RAMP SEPARATIONS IS REFERRED TO THE NORMAL EDGE OF THE MAINLINE ROADWAY.

OVERHEAD SIGNS

1. VERTICAL ALIGNMENT
POSITION PANELS FOR ALL OVERHEAD STRUCTURES SO THAT PANEL FACE IS PLUMB.
2. OVERHEAD SIGN STRUCTURES SHALL NOT BE ERECTED WITHOUT ATTACHING LUMINAIRES, SUPPORTS, AND/OR SIGNS.
3. HORIZONTAL ALIGNMENT
 - A) POSITION ALL OVERHEAD SIGNS SO THAT THE FACE OF THE PANEL IS AT RIGHT ANGLES TO THE NORMAL EDGE OF ROADWAY, IF ON A STRAIGHT ROADWAY SECTION.
 - B) POSITION ALL OVERHEAD SIGNS SO THAT THE FACE OF THE PANEL IS AT RIGHT ANGLES TO THE TANGENT OF THE CURVE AT SIGN LOCATION, IF ON A HORIZONTAL CURVE.
 - C) POSITIONING OF SIGNS AT GORES AND RAMP SEPARATIONS IS REFERRED TO THE NORMAL EDGE OF THE MAINLINE ROADWAY.
4. VERTICAL CLEARANCE
 - A) OVERHEAD SIGNS SHALL HAVE A MINIMUM VERTICAL CLEARANCE OF 17'-9" FROM ROADWAY TO THE BOTTOM OF LIGHT FIXTURES. ALL LIGHT FIXTURES ARE TO BE AT THE SAME ELEVATION.
 - B) IF THE CONTRACTOR CANNOT OBTAIN 17'-9" (SEE 3A) CLEARANCE, HE IS TO CEASE WORK AND CONTACT THE PROJECT ENGINEER FOR FURTHER INSTRUCTIONS. THE PROJECT ENGINEER MAY CONTACT THE TRAFFIC ENGINEERING DESIGN DIVISION FOR ASSISTANCE.
 - C) ON ALL OVERHEAD SIGNS, THE MINIMUM CLEARANCE TO BOTTOM OF DESIGN SIGN: 20'-9".

PROJECT REQUIREMENTS

ALL NEW SIGNS ON THIS PROJECT SHALL BE FABRICATED FROM SHEETING WHICH MEETS ALL OF THE FOLLOWING REQUIREMENTS, UNLESS OTHERWISE SPECIFIED IN THE CONTRACT DOCUMENTS, OR AS DIRECTED BY THE ENGINEER:

1. SHEETING SHALL MEET THE REQUIREMENTS OF SECTIONS 813 AND 950.03 OF MDOT SHA'S STANDARD SPECIFICATIONS FOR CONSTRUCTION AND MATERIALS 2017 EDITION AND SUBSEQUENT REVISIONS AND SUPPLEMENTS.
2. LISTED ON MDOT SHA OFFICE OF TRAFFIC AND SAFETY'S QUALIFIED PRODUCTS LIST (QPL).

PROJECT REQUIREMENTS CONT'D

3. THE FOLLOWING TYPES OF SHEETING SHALL BE USED FOR THE SPECIFIED SIGN CLASSIFICATIONS:

GENERAL NOTE: ALL COLORS SHALL BE RETROREFLECTIVE EXCEPT BLACK. BLACK TEXT, BORDERS, SYMBOLS OR ANY BLACK ELEMENTS OF ANY SIGN SHALL BE NON-REFLECTIVE. THIS APPLIES TO ALL MDOT SHA SIGNS AS SHOWN BELOW.

A) GUIDE, EXIT GORE, GENERAL INFORMATION, AND SERVICE SIGNS - FALL INTO TWO SUB CATEGORIES:

(I). GROUND MOUNTED:
ALL RETROREFLECTIVE SHEETING ELEMENTS OF THESE SIGNS SHALL MEET OR EXCEED THE REQUIREMENTS FOR ASTM TYPE IX (9).

(II). OVERHEAD STRUCTURE SIGNS AND OVERHEAD CANTILEVER SIGNS:
ALL RETROREFLECTIVE SHEETING ELEMENTS OF ALL OVERHEAD SIGNS SHALL MEET OR EXCEED THE REQUIREMENTS FOR ASTM TYPE XI(II). (THIS SECTION DOES NOT APPLY TO OVERHEAD SIGNALIZED INTERSECTION SIGNING; MAST ARM OR SPAN WIRE. FOLLOW THE REQUIREMENTS FOR THE RESPECTIVE SIGN CLASSIFICATION FOR SIGNAL SIGNING.)

B) WARNING SIGNS - RETROREFLECTIVE SHEETING FOR WARNING SIGNS (FLUORESCENT YELLOW AND FLUORESCENT ORANGE) SHALL MEET OR EXCEED THE REQUIREMENTS FOR ASTM TYPE IX (9). REGULATORY MESSAGES WITHIN WARNING SIGNS SHALL FOLLOW THE REQUIREMENTS FOR REGULATORY SIGNS.

C) SCHOOL SIGNS - RETROREFLECTIVE SHEETING FOR SCHOOL SIGNS (FLUORESCENT YELLOW AND FLUORESCENT YELLOW-GREEN) SHALL MEET OR EXCEED THE REQUIREMENTS FOR ASTM TYPE IX (9). REGULATORY MESSAGES WITHIN SCHOOL SIGNS SHALL FOLLOW THE REQUIREMENTS FOR REGULATORY SIGNS.

D) REGULATORY SIGNS - FALL INTO THREE SUBCATEGORIES:

(I). "RED" REGULATORY SIGNS; (SPECIFICALLY - STOP, YIELD, DO NOT ENTER AND WRONG WAY). ALL RETROREFLECTIVE SHEETING ELEMENTS OF THESE SIGNS SHALL MEET OR EXCEED THE REQUIREMENTS FOR ASTM TYPE IX (9).

(II). ALL R7 AND R8 SERIES PARKING RELATED SIGNS AND THEIR SUPPLEMENTAL PANELS, NO TRESPASSING SIGNS, AND SIGNS DIRECTED AT PEDESTRIANS AND BICYCLISTS ONLY. ALL RETROREFLECTIVE SHEETING ELEMENTS OF THESE SIGNS SHALL MEET THE REQUIREMENTS FOR ASTM TYPE IV (4).

(III). ALL OTHER REGULATORY SIGNS - ALL RETROREFLECTIVE SHEETING ELEMENTS OF THESE SIGNS SHALL MEET ASTM TYPE IV (4) INCLUDING RED ELEMENTS. WARNING MESSAGES WITHIN REGULATORY SIGNS SHALL FOLLOW THE REQUIREMENTS FOR WARNING SIGNS.

E) ROUTE MARKERS (INDEPENDENT USE AND GUIDE SIGN USE)

INDEPENDENT USE: ALL RETROREFLECTIVE SHEETING ELEMENTS OF THESE SIGNS SHALL MEET BUT NOT TO EXCEED THE REQUIREMENTS FOR ASTM TYPE IV (4).

GUIDE SIGN USE: WHEN INCORPORATED IN THE BODY OF A GUIDE SIGN, ALL RETROREFLECTIVE SHEETING ELEMENTS OF THESE SIGNS SHALL MEET THE SHEETING REQUIREMENTS OF THE GUIDE SIGNS FOR WHICH THEY ARE TO BE APPLIED; GROUND MOUNT ASTM TYPE IX (9) OR OVERHEAD ASTM TYPE XI(II).

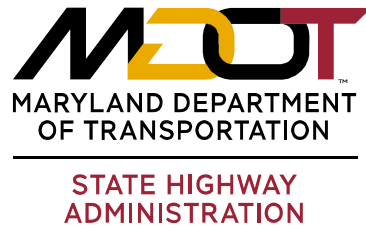
F) LOGOS AND / OR GRAPHICS - WITHIN SIGNS SHALL FOLLOW THE REQUIREMENTS FOR THE RESPECTIVE SIGN CLASSIFICATION UNLESS OTHERWISE SPECIFIED IN THE CONTRACT DOCUMENTS, OR AS DIRECTED BY THE ENGINEER.

G) SPECIFIC SERVICE (LOGO) SIGNING - ALL COPY, DIVIDER BORDERS, LOGOS AND ARROWS SHALL BE DEMOUNTABLE ALUMINUM OVERLAYS. .032 MINIMUM TO .063 MAXIMUM. ALL RETROREFLECTIVE SHEETING ELEMENTS OF THESE SIGNS SHALL MEET OR EXCEED THE REQUIREMENTS FOR ASTM TYPE IX (9). DISTANCES ON DIRECTIONAL ARROWS WHEN SPECIFIED SHALL BE BLACK. THE OVERLAYS ARE TO BE APPLIED WITH .125 ALUMINUM POP RIVETS TO THE BODY OF THE MAIN SIGN.

H) CIVIL DEFENSE SIGNS AND OTHER SIGNS - NOT SPECIFICALLY FALLING INTO ONE OF THE CATEGORIES ABOVE, SHALL FOLLOW THE GUIDELINES FOR THE SIGN CLASSIFICATION THAT MOST CLOSELY MATCHES THE COLOR(S) OF THE PROPOSED SIGN.

4. THE FOLLOWING MINIMUM THICKNESS SHALL BE USED FOR THE APPROPRIATE WIDTH OF SHEET ALUMINUM BLANKS:

LONGEST DIMENSION	MINIMUM THICKNESS
UP TO 12"	0.040"
GREATER THAN 12" TO 24"	0.063"
GREATER THAN 24" TO 36"	0.080"
GREATER THAN 36" TO 48"	0.100"
OVER 48"	0.125"



OFFICE OF TRAFFIC & SAFETY
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NEW AVE BIKEWAY
SECTION B
MD 650 (NEW HAMPSHIRE AVE) FROM
POPLAR AVE TO AUBURN AVE

STATE HIGHWAY
ADMINISTRATION

GENERAL NOTES AND PROPOSALS

SCALE NONE DATE NOVEMBER 2021 CONTRACT NO. 20-AP-MQ-020-XX

DESIGNED BY SJL COUNTY PRINCE GEORGE'S
DRAWN BY SJL LOGMILE 16065000.43 TO 00.81
CHECKED BY WFW TMS NO. PENDING
MDE/PRD SEE TITLE SHEET TOD NO.

DRAWING NO. SN-1 OF 1 SHEET NO. 35 OF 42

PLOTTED: 10/28/2021
FILE: \\ad.rnk.com\ys\Cloud\Projects\2020\20007_NewAveSecB\CADD\Plans\psN-1001_NewAveBike_B.dgn

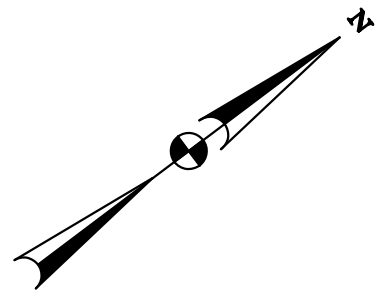


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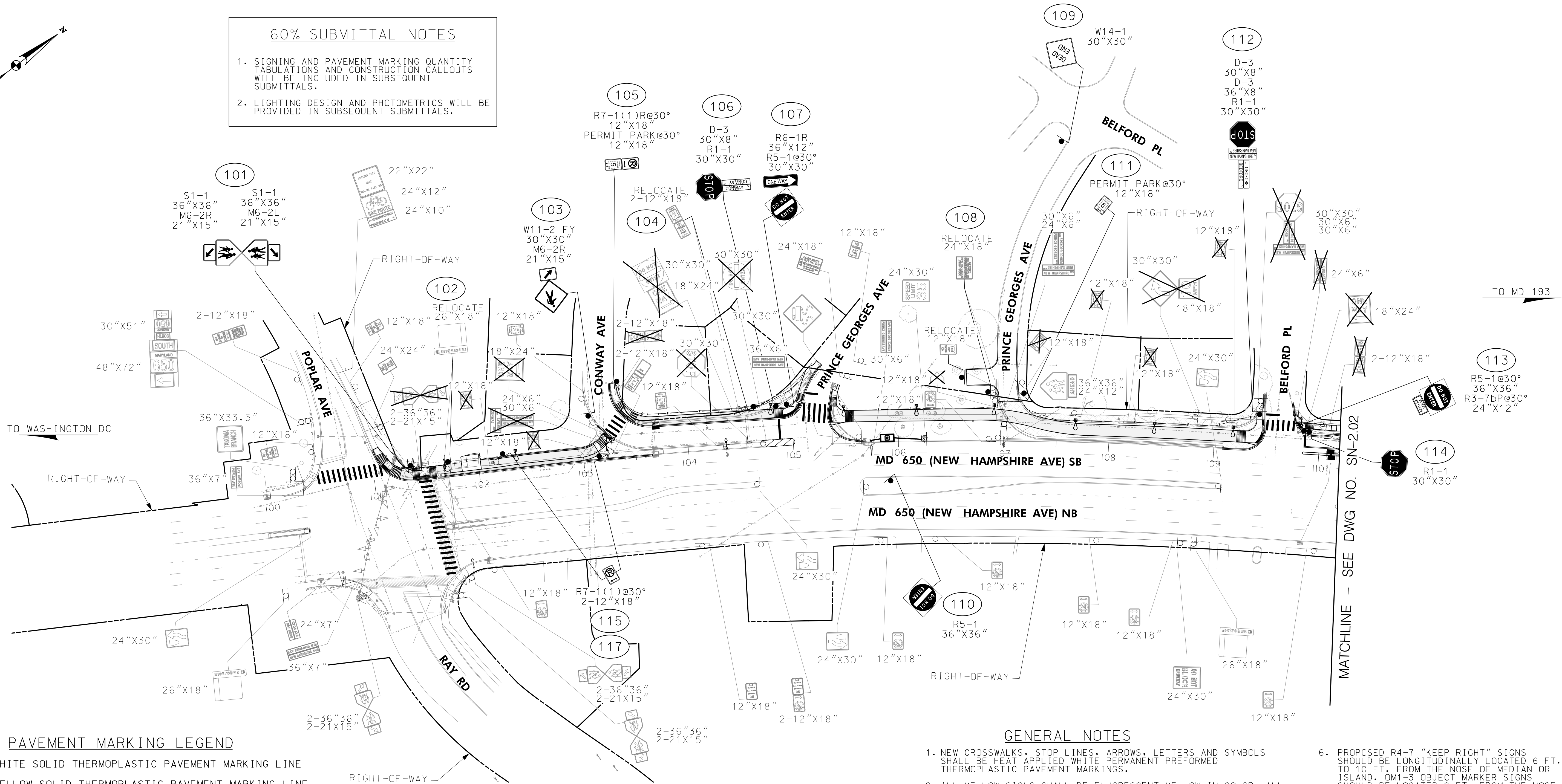
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60% SUBMITTAL NOTES

- SIGNING AND PAVEMENT MARKING QUANTITY TABULATIONS AND CONSTRUCTION CALLOUTS WILL BE INCLUDED IN SUBSEQUENT SUBMITTALS.
- LIGHTING DESIGN AND PHOTOMETRICS WILL BE PROVIDED IN SUBSEQUENT SUBMITTALS.



PAVEMENT MARKING LEGEND

- A. 5 IN. WHITE SOLID THERMOPLASTIC PAVEMENT MARKING LINE
- B. 5 IN. YELLOW SOLID THERMOPLASTIC PAVEMENT MARKING LINE
- C. 5 IN. WHITE SKIP THERMOPLASTIC PAVEMENT MARKING LINE (10 FT. LINE, 30 FT. GAP)
- D. 5 IN. WHITE SKIP THERMOPLASTIC PAVEMENT MARKING LINE (3 FT. LINE, 9 FT. GAP)
- E. 24 IN. SOLID WHITE PERMANENT PREFORMED THERMOPLASTIC PAVEMENT MARKING LINE
- F. 16 IN. WHITE PERMANENT PREFORMED THERMOPLASTIC PAVEMENT MARKING LINE
- G. WHITE PERMANENT PREFORMED THERMOPLASTIC PAVEMENT MARKING SYMBOL
- H. GREEN MMA BICYCLE CONFLICT MARKINGS. SEE DETAILS ON SN-2.02
- J. REMOVE EXISTING PAVEMENT MARKING LINE, SYMBOL OR ARROW BY HYDROBLASTING

± RECESSED SNOWPLOWABLE RAISED PAVEMENT MARKER WITH PLASTIC HOLDER

LEGEND

- EXISTING SIGN TO REMAIN
- EXISTING SIGN TO BE REMOVED
- PROPOSED SIGN
- EXISTING GROUND MOUNTED SIGN
- PROPOSED GROUND MOUNTED SIGN
- PROPOSED PEDESTRIAN LIGHT POLE
- PROPOSED LEASED LIGHT ON UTILITY POLE

GENERAL NOTES

- NEW CROSSWALKS, STOP LINES, ARROWS, LETTERS AND SYMBOLS SHALL BE HEAT APPLIED WHITE PERMANENT PREFORMED THERMOPLASTIC PAVEMENT MARKINGS.
- ALL YELLOW SIGNS SHALL BE FLUORESCENT YELLOW IN COLOR. ALL SCHOOL SIGNS SHALL BE FLUORESCENT YELLOW GREEN IN COLOR.
- UNDERGROUND UTILITIES SHOWN ON THESE PLANS ARE SCHEMATIC ONLY AND MAY NOT BE COMPLETE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING "MISS UTILITY" PRIOR TO CONSTRUCTION SO THAT ALL UTILITIES MAY BE LOCATED IN THE FIELD. IF THE CONTRACTOR PERCEIVES THAT A CONFLICT BETWEEN THE UTILITIES AND THE SIGNS WILL OCCUR, THE CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY SO THAT THE CONFLICT MAY BE RESOLVED.
- NEW PAVEMENT MARKINGS SHALL TRANSITION TO TIE INTO EXISTING MARKINGS AS DIRECTED BY THE ENGINEER.
- PROPOSED R4-7 "KEEP RIGHT" SIGNS SHOULD BE LONGITUDINALLY LOCATED 6 FT. TO 10 FT. FROM THE NOSE OF MEDIAN OR ISLAND. OM1-3 OBJECT MARKER SIGNS SHOULD BE LOCATED 2 FT. FROM THE NOSE OF MEDIAN OR ISLAND, PER MDMUTC0 FIGURE 2B-10b.
- RIGHT-OF-WAY SHOWN ON THE PLANS IS APPROXIMATE AND BASED ON THE BEST AVAILABLE INFORMATION.



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NEW AVE BIKEWAY
SECTION B
MD 650 (NEW HAMPSHIRE AVE) FROM
POPLAR AVE TO AUBURN AVE

SIGNING & PAVEMENT MARKING PLAN

SCALE 1" = 50' DATE NOVEMBER 2021 CONTRACT NO. 20-AP-MQ-020-XX

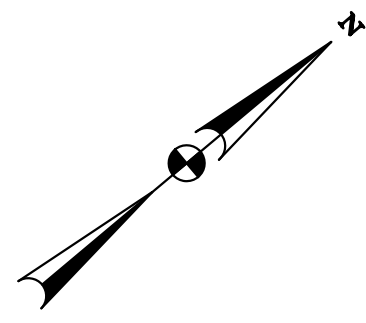
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CHECKED BY WFW TMS NO. PENDING
MDE/PRD SEE TITLE SHEET TOD NO.

DRAWING NO. SN-2.01 OF 2 SHEET NO. 36 OF 42



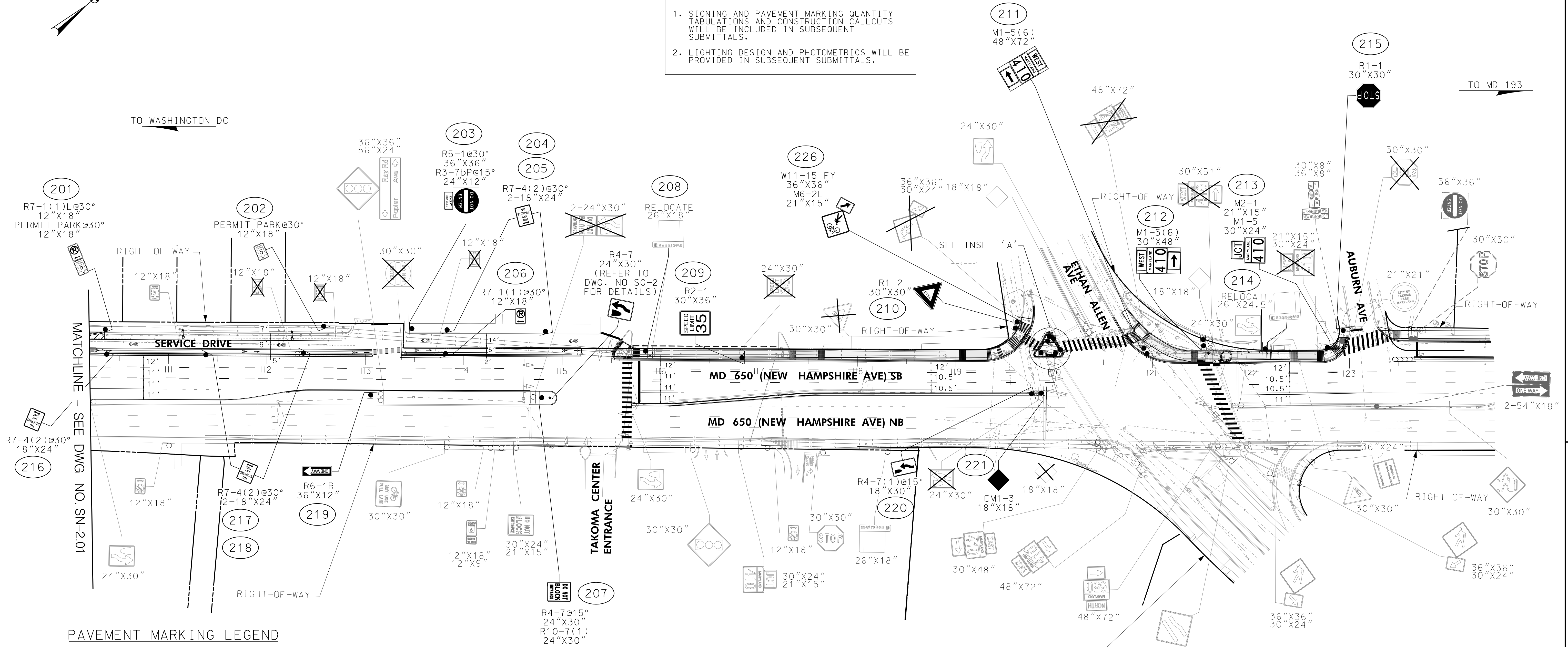
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BY: WFW



60% SUBMITTAL NOTES

- SIGNING AND PAVEMENT MARKING QUANTITY TABULATIONS AND CONSTRUCTION CALLOUTS WILL BE INCLUDED IN SUBSEQUENT SUBMITTALS.
- LIGHTING DESIGN AND PHOTOMETRICS WILL BE PROVIDED IN SUBSEQUENT SUBMITTALS.

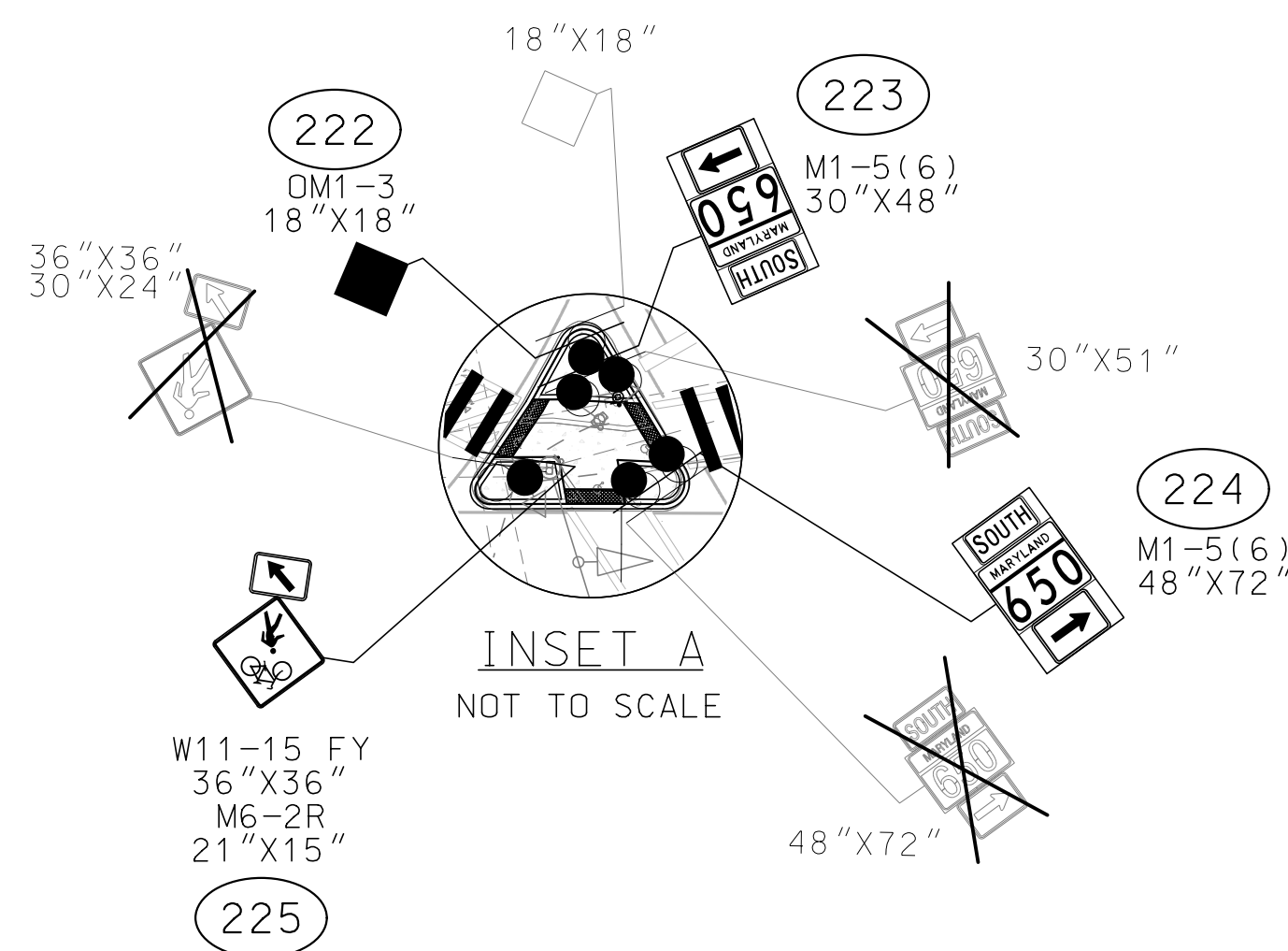


PAVEMENT MARKING LEGEND

- A. 5 IN. WHITE SOLID THERMOPLASTIC PAVEMENT MARKING LINE
- B. 5 IN. YELLOW SOLID THERMOPLASTIC PAVEMENT MARKING LINE
- C. 5 IN. WHITE SKIP THERMOPLASTIC PAVEMENT MARKING LINE (10 FT. LINE, 30 FT. GAP)
- D. 5 IN. WHITE SKIP THERMOPLASTIC PAVEMENT MARKING LINE (3 FT. LINE, 9 FT. GAP)
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- F. 16 IN. WHITE PERFORMED THERMOPLASTIC PAVEMENT MARKING LINE
- G. WHITE PERFORMED THERMOPLASTIC PAVEMENT MARKING SYMBOL
- H. GREEN MMA BICYCLE CONFLICT MARKINGS. SEE DETAILS ON SN-2.02
- J. REMOVE EXISTING PAVEMENT MARKING LINE, SYMBOL OR ARROW BY HYDROBLASTING

± RECESSED SNOWPLOWABLE RAISED PAVEMENT MARKER WITH PLASTIC HOLDER

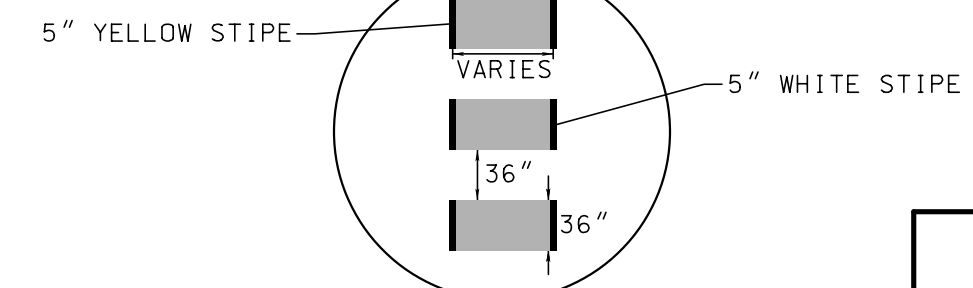
INSET A NOT TO SCALE



LEGEND

- EXISTING SIGN TO REMAIN
- EXISTING SIGN TO BE REMOVED
- PROPOSED SIGN
- EXISTING GROUND MOUNTED SIGN
- PROPOSED GROUND MOUNTED SIGN
- PROPOSED PEDESTRIAN LIGHT POLE
- PROPOSED LEASED LIGHT ON UTILITY POLE

GREEN MMA BICYCLE CONFLICT MARKINGS DETAIL



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NEW AVE BIKEWAY
SECTION B
MD 650 (NEW HAMPSHIRE AVE) FROM
POPLAR AVE TO AUBURN AVE

SIGNING & PAVEMENT MARKING PLAN

DESIGNED BY	SJL	COUNTY	PRINCE GEORGE'S
DRAWN BY	SJL	LOGMILE	16065000.43 TO .00.81
CHECKED BY	WFW	TMS NO.	PENDING
MDE/PRD	SEE TITLE SHEET	TOD NO.	
DRAWING NO.	SN-2.02	OF	2
SHEET NO.	37	OF	42

PLOTTED: 10/28/2021
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BY: WFW

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TAKOMA PARK

MARYLAND
410

MONTGOMERY
COUNTY

ETHAN ALLEN AVE

TS-04

MARYLAND
650

MARYLAND
410

EAST

WEST

HWY

TS-03

TS-02

PRINCE GEORGES COUNTY

TS-01

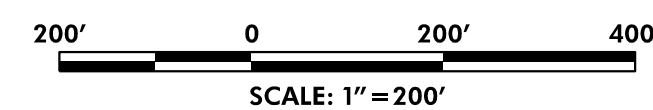
MARYLAND
650



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60% PLANS

NOVEMBER 2021

CITY OF TAKOMA PARK
NEW AVE BIKEWAY, SECTION B
MD 650 (NEW HAMPSHIRE AVENUE)
POPLAR AVE TO AUBURN AVE
SHA TRACKING NO. 20-AP-MO-020-XX

TREE SAVE KEY

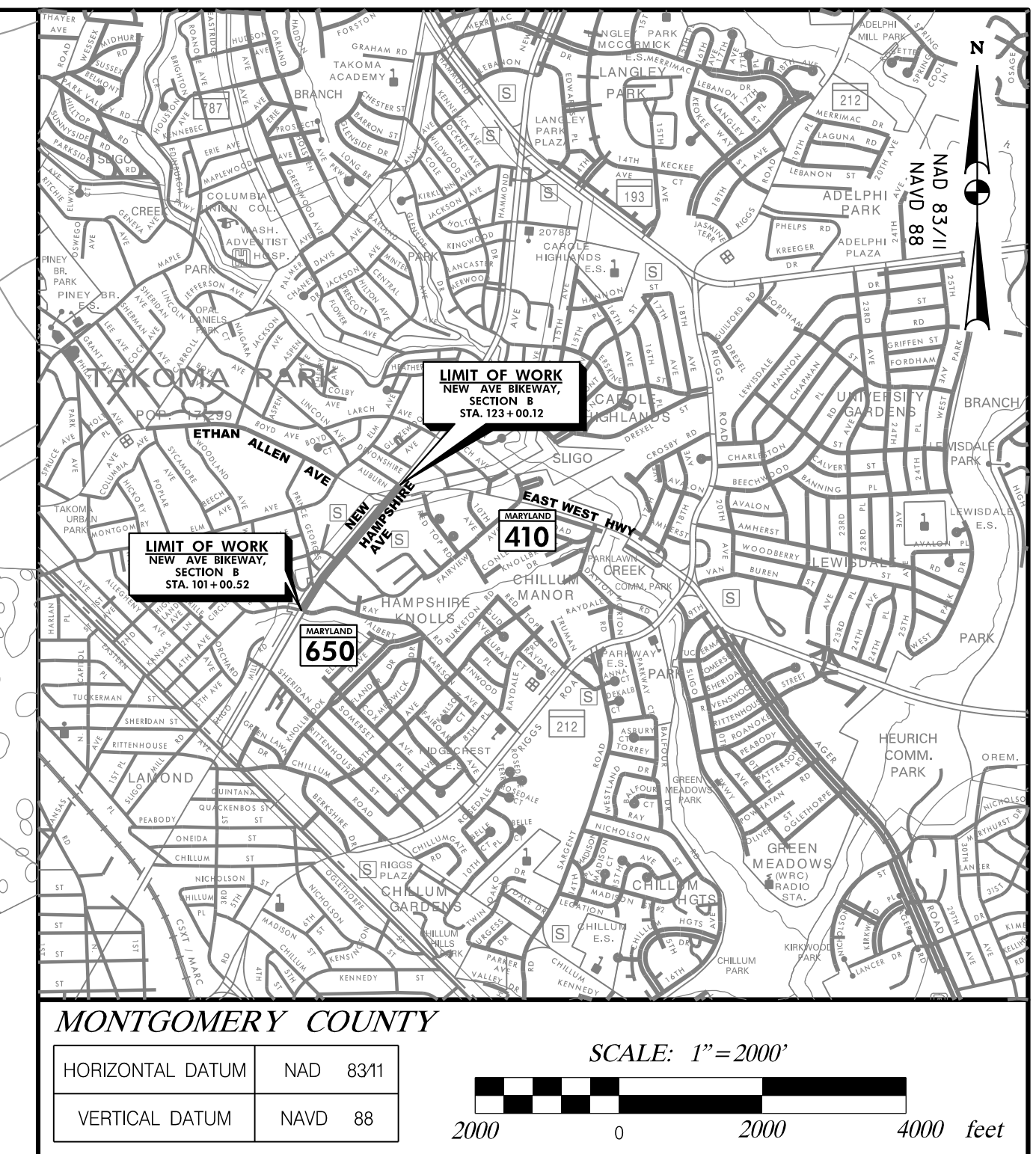
SCALE 1"=200' DATE NOVEMBER 2021 CONTRACT NO. T.B.D.

DESIGNED BY SJK COUNTY MONTGOMERY

DRAWN BY KBJ

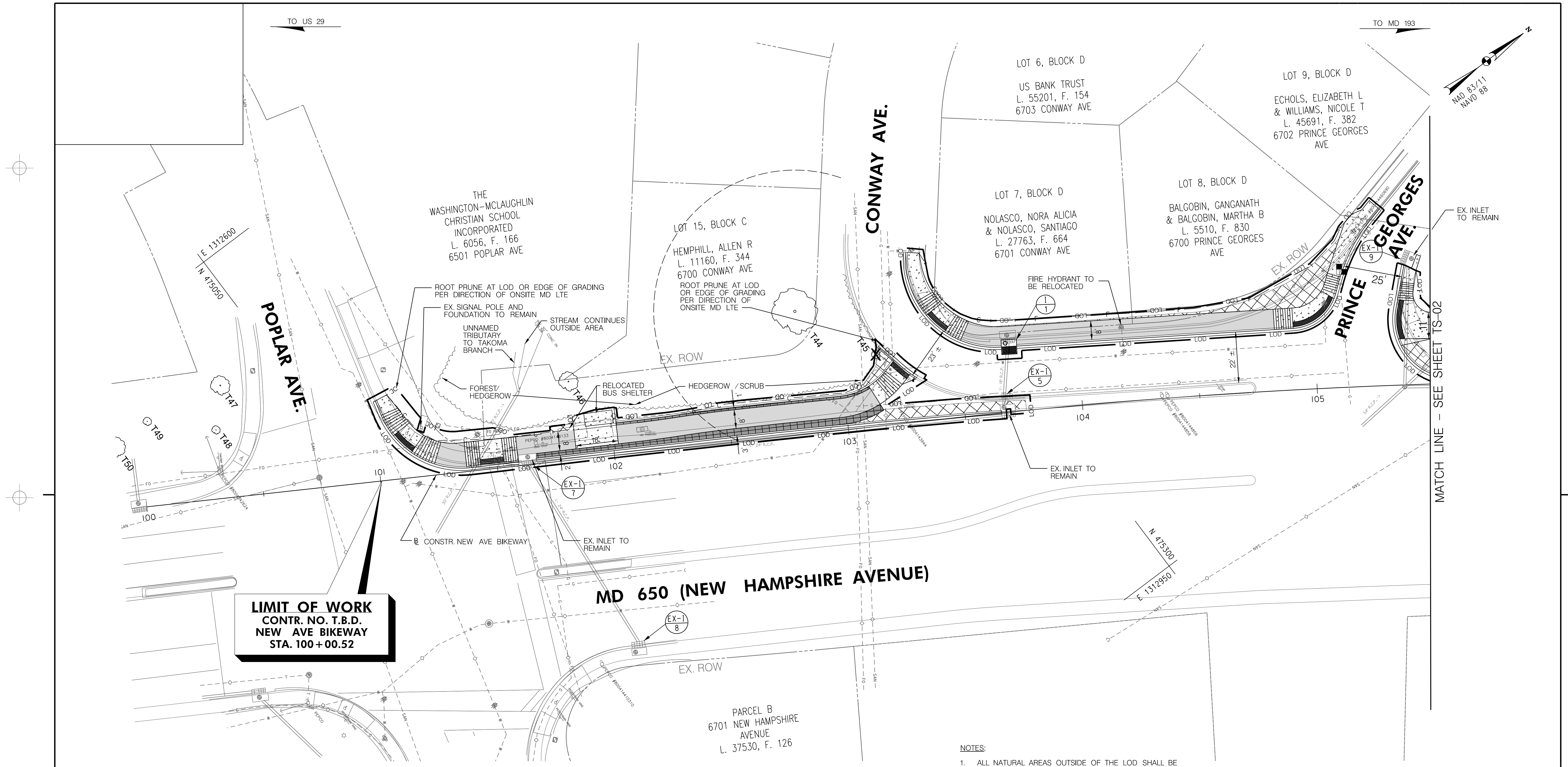
F.A.P. NO. _____ T.B.D. _____

DRAWING NO. TS-KEY	1 OF 1	SHEET NO.	38 OF 44
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S-KEY

BY: kboampong -



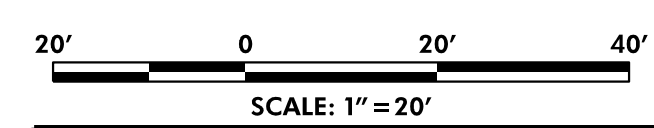
LIMIT OF WORK
CONTR. NO. T.B.D.
NEW AVE BIKEWAY
STA. 100 + 00.52

- NOTES:
1. ALL NATURAL AREAS OUTSIDE OF THE LOD SHALL BE CONSIDERED TREE PRESERVATION AREAS TO BE LEFT UNDISTURBED.
 2. ALL TREE CARE RELATED ACTIVITIES, SUCH AS ROOT PRUNING SHALL BE PERFORMED OR DIRECTLY SUPERVISED BY A MD LTE.
 3. TREE PLANTING TO BE INCLUDED ON FINAL PLANS.

07/21/2021
DATE

MARYLAND DNR QUALIFIED PROFESSIONAL

- LEGEND
- | | | | |
|--|---|--|--|
| | EXISTING TREE LINE / INDIVIDUAL TREE CANOPY | | 5 INCH CONCRETE SIDEWALK |
| | EXISTING SPECIMEN TREE (≥30" DBH) WITH CRITICAL ROOT ZONE (CRZ) | | CONCRETE DRIVEWAY |
| | EXISTING TREE (<30" DBH) | | SPECIALTY PAVER – TYPE 2 |
| | TREE TO BE REMOVED | | ASPHALT SHARED USE PATH |
| | LOD — LIMITS OF DISTURBANCE | | FULL DEPTH PAVING |
| | SF — SILT FENCE | | SIDEWALK AND PAVEMENT REMOVAL (PAID FOR AS CLASS 1 EXCAVATION) |



NO.	REVISION	DATE	BY

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NOVEMBER 2021

CITY OF TAKOMA PARK
NEW AVE BIKEWAY, SECTION B
MD 650 (NEW HAMPSHIRE AVENUE)
POPLAR AVE TO AUBURN AVE
SHA TRACKING NO. 20-AP-MO-020-XX

TREE SAVE PLAN

SCALE 1"=20' DATE NOVEMBER 2021 CONTRACT NO. T.B.D.

DESIGNED BY SJK COUNTY MONTGOMERY
DRAWN BY EMT
CHECKED BY GRO
F.A.P. NO. T.B.D.

DRAWING NO. TS-01 1 OF 6 SHEET NO. 39 OF 44

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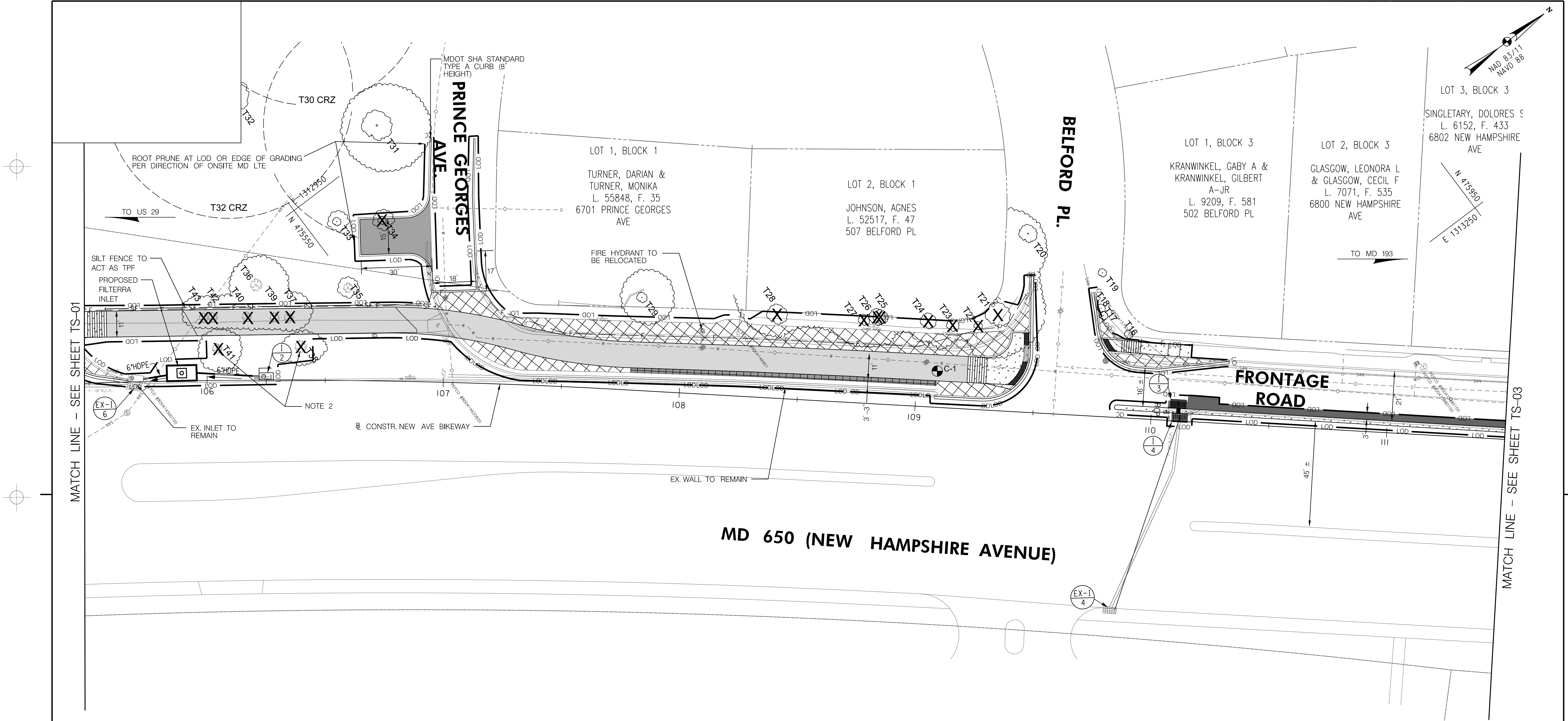
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TS-02

- NOTES:
1. ALL NATURAL AREAS OUTSIDE OF THE LOD SHALL BE CONSIDERED TREE PRESERVATION AREAS TO BE LEFT UNDISTURBED.
 2. ALL TREE CARE RELATED ACTIVITIES, SUCH AS ROOT PRUNING SHALL BE PERFORMED OR DIRECTLY SUPERVISED BY A MD LTE.
 3. TREE PLANTING TO BE INCLUDED ON FINAL PLANS.

MARYLAND DNR QUALIFIED PROFESSIONAL
07/21/2021
DATE

LEGEND

	EXISTING TREE LINE / INDIVIDUAL TREE CANOPY		5 INCH CONCRETE SIDEWALK
	EXISTING SPECIMEN TREE (≥30" DBH) WITH CRITICAL ROOT ZONE (CRZ)		CONCRETE DRIVEWAY
	EXISTING TREE (<30" DBH)		SPECIALTY PAVER – TYPE 2
	TREE TO BE REMOVED		ASPHALT SHARED USE PATH
	LOD — LIMITS OF DISTURBANCE		FULL DEPTH PAVING
	SF — SILT FENCE		SIDEWALK AND PAVEMENT REMOVAL (PAID FOR AS CLASS 1 EXCAVATION)

20' 0 20' 40'
SCALE: 1" = 20'

NO.	REVISION	DATE	BY

60% PLANS
NOVEMBER 2021

CITY OF TAKOMA PARK
NEW AVE BIKEWAY, SECTION B
MD 650 (NEW HAMPSHIRE AVENUE)
POPLAR AVE TO AUBURN AVE
SHA TRACKING NO. 20-AP-MO-020-XX

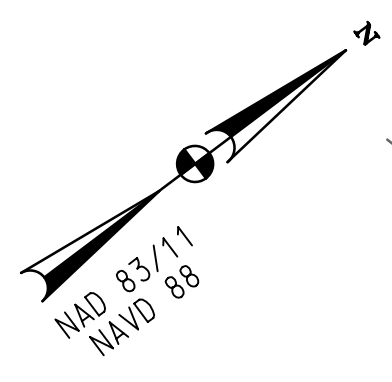
TREE SAVE PLAN

SCALE	1"=20'	DATE	NOVEMBER 2021	CONTRACT NO.	T.B.D.
DESIGNED BY	SJK	COUNTY	MONTGOMERY		
DRAWN BY	EMT				
CHECKED BY	GRO				
F.A.P. NO.	T.B.D.				
DRAWING NO.	TS-02	2 OF 6	SHEET NO.	40 OF 44	

PLOTTED: 10/26/2021
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E 1313250
N 476050

N 476400
E 1313600

TO MD 193

MATCH LINE - SEE SHEET TS-02

MATCH LINE - SEE SHEET TS-04

LOT 4, BLOCK 3

BEATRICE CORDERO
ACEA & ERNESTO ACEA
L. 8991, F. 135
6804 NEW HAMPSHIRE
AVE

TO US 29

LOT 5, BLOCK 3

DIVINE C. GANA &
JONAS M. MUWUD
L. 45717, F. 271
6806 NEW HAMPSHIRE
AVE

PARCEL P

NEW HAMPSHIRE
AVENUE SHOPPING
CENTER LLC
L. 49704, F. 279
6822 NEW HAMPSHIRE
AVE

FRONTAGE
ROAD

MD 650 (NEW HAMPSHIRE AVENUE)

CONSTR. NEW AVE BIKEWAY

PROPOSED TRAFFIC
SIGNAL RELOCATION

LIGHT POLE TO BE
REMOVED (SEE
NOTE 3)

UTILITY POLES (TO BE
RELOCATED BY OTHERS)

UTILITY POLE AND
GUY WIRE (TO BE
RELOCATED BY
OTHERS)

NOTES:

1. ALL NATURAL AREAS OUTSIDE OF THE LOD SHALL BE CONSIDERED TREE PRESERVATION AREAS TO BE LEFT UNDISTURBED.
2. ALL TREE CARE RELATED ACTIVITIES, SUCH AS ROOT PRUNING SHALL BE PERFORMED OR DIRECTLY SUPERVISED BY A MD L.T.E.
3. TREE PLANTING TO BE INCLUDED ON FINAL PLANS.

MARYLAND DNR QUALIFIED PROFESSIONAL

07/21/2021
DATE

LEGEND

- T44
- T40
-
- LOD
- SF
- EXISTING TREE LINE /
INDIVIDUAL TREE CANOPY
- EXISTING SPECIMEN TREE
(≥30" DBH) WITH CRITICAL
ROOT ZONE (CRZ)
- EXISTING TREE (<30" DBH)
- TREE TO BE REMOVED
- LIMITS OF DISTURBANCE
- SILT FENCE
-
- 5 INCH CONCRETE SIDEWALK
-
- CONCRETE DRIVEWAY
-
- SPECIALTY PAVER - TYPE 2
-
- ASPHALT SHARED USE PATH
-
- FULL DEPTH PAVING
-
- SIDEWALK AND PAVEMENT REMOVAL
(PAID FOR AS CLASS 1 EXCAVATION)

20' 0 20' 40'
SCALE: 1" = 20'

NO.	REVISION	DATE	BY

60% PLANS
NOVEMBER 2021

CITY OF TAKOMA PARK
NEW AVE BIKEWAY, SECTION B
MD 650 (NEW HAMPSHIRE AVENUE)
POPLAR AVE TO AUBURN AVE
SHA TRACKING NO. 20-AP-MO-020-XX

TREE SAVE PLAN

SCALE 1"=20' DATE NOVEMBER 2021 CONTRACT NO. T.B.D.

DESIGNED BY SJK COUNTY MONTGOMERY
DRAWN BY EMT
CHECKED BY GRO
F.A.P. NO. T.B.D.

DRAWING NO. TS-03 3 OF 6 SHEET NO. 41 OF 44

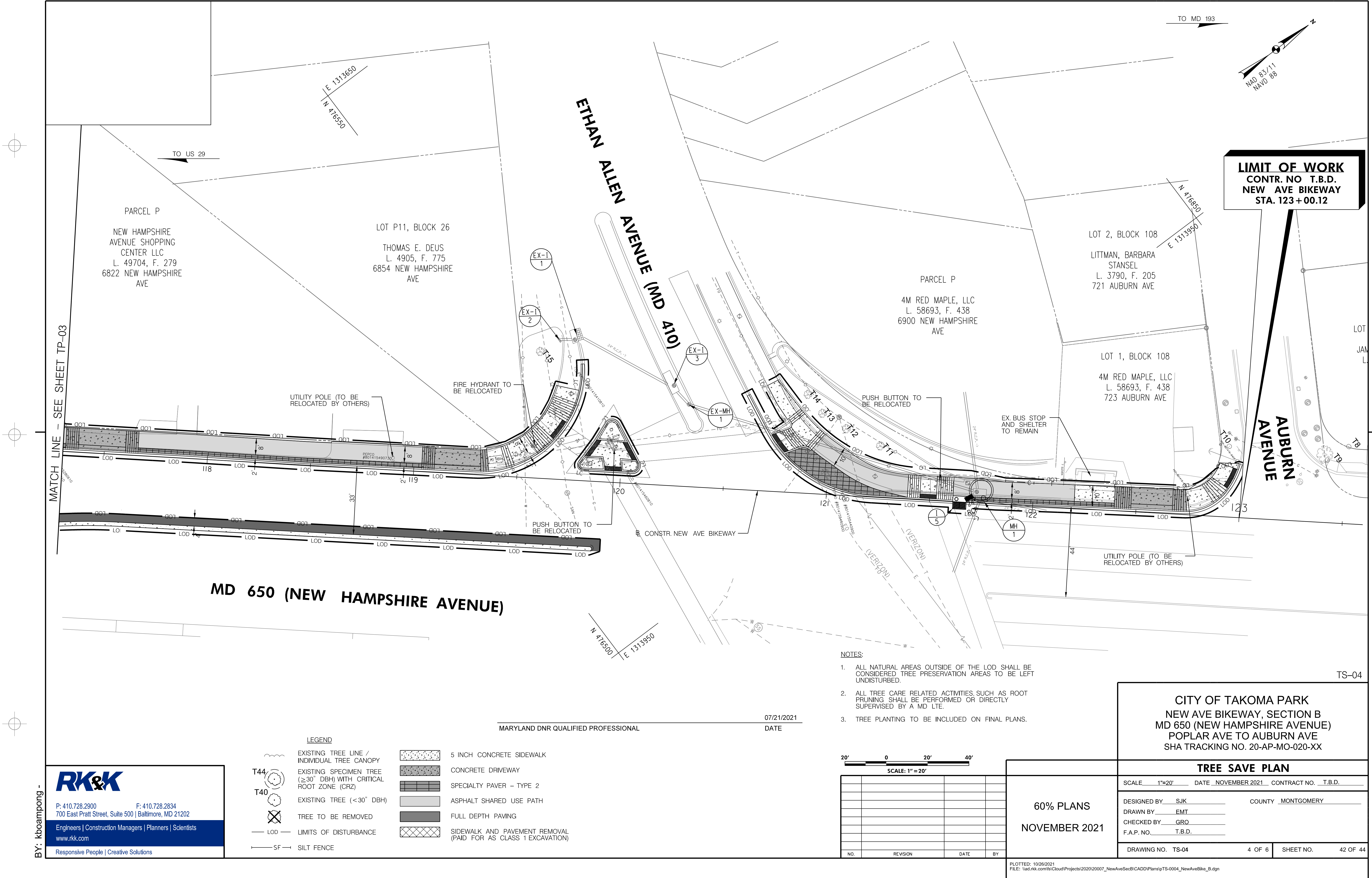
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FILE: \\ad.rkk.com\ts\Cloud\Projects\2020\20007_NewAveSecB\CADD\Plans\pTS-0003_NewAveBike_B.dgn

BY: kboampong -

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LIMIT OF WORK
CONTR. NO T.B.D.
NEW AVE BIKEWAY
STA. 123+00.12

MATCH LINE - SEE SHEET TP-03

MD 650 (NEW HAMPSHIRE AVENUE)

ETHAN ALLEN AVENUE (MD 410)

AUBURN AVENUE

- NOTES:**
1. ALL NATURAL AREAS OUTSIDE OF THE LOD SHALL BE CONSIDERED TREE PRESERVATION AREAS TO BE LEFT UNDISTURBED.
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07/21/2021
DATE

MARYLAND DNR QUALIFIED PROFESSIONAL

LEGEND

	EXISTING TREE LINE / INDIVIDUAL TREE CANOPY		5 INCH CONCRETE SIDEWALK
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	SF - SILT FENCE		SIDEWALK AND PAVEMENT REMOVAL (PAID FOR AS CLASS 1 EXCAVATION)

20' 0 20' 40'
SCALE: 1" = 20'

NO.	REVISION	DATE	BY

60% PLANS
NOVEMBER 2021

CITY OF TAKOMA PARK
NEW AVE BIKEWAY, SECTION B
MD 650 (NEW HAMPSHIRE AVENUE)
POPLAR AVE TO AUBURN AVE
SHA TRACKING NO. 20-AP-MO-020-XX

TREE SAVE PLAN

SCALE 1"=20' DATE NOVEMBER 2021 CONTRACT NO. T.B.D.

DESIGNED BY SJK COUNTY MONTGOMERY
DRAWN BY EMT
CHECKED BY GRO
F.A.P. NO. T.B.D.

DRAWING NO. TS-04 4 OF 6 SHEET NO. 42 OF 44

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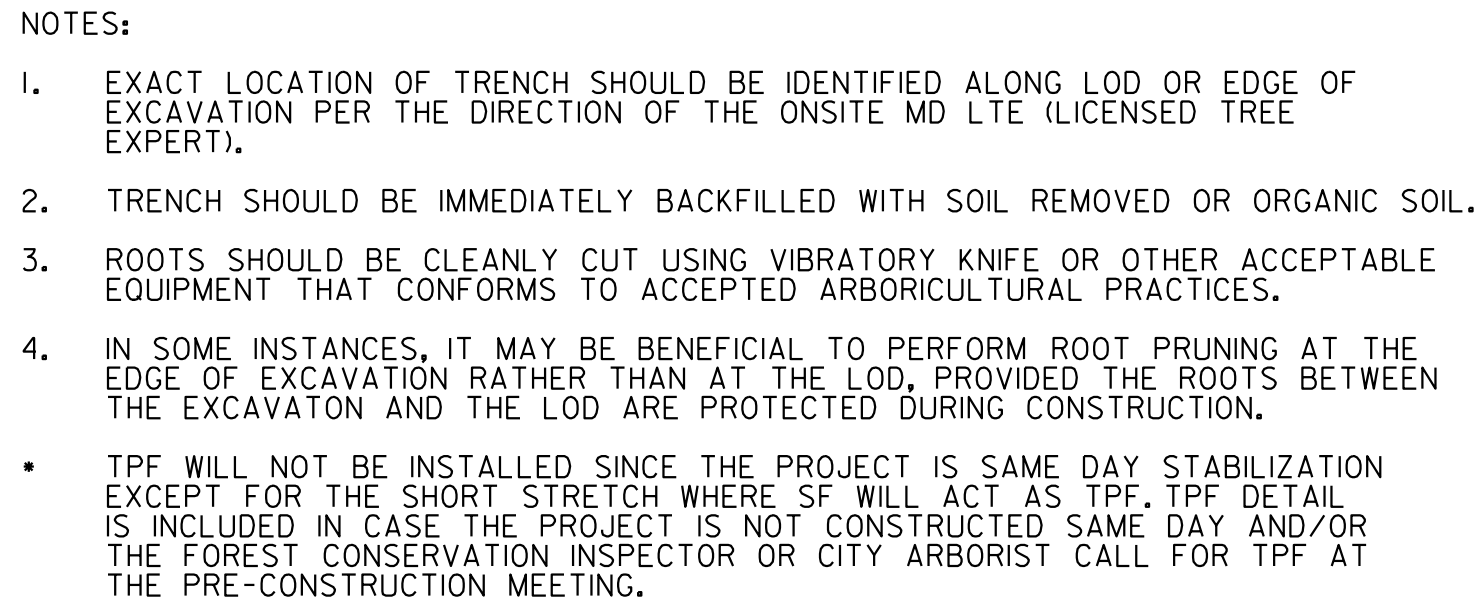
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NOT TO SCALE

Source: Adapted from Steve Clark & Associates/ACRT, Inc, and Forest Conservation Manual, 1991

Bold = Specimen Trees ($\geq 30"$ DBH) * Shrub in road ROW
THERE ARE NO SIGNIFICANT TREES ($\geq 24"$ DBH) THAT ARE NOT ALSO SPECIMEN TREES ($\geq 30"$ DBH). ** Outside area shown on plans

IMPACT NOTE:

NO FOREST WILL BE CLEARED, AND NO SIGNIFICANT OR SPECIMEN TREES WILL BE REMOVED.

	TREE SAVE NOTES AND DETAILS		
60% PLANS NOVEMBER 2021	SCALE <u>1"=20'</u> DATE <u>NOVEMBER 2021</u> CONTRACT NO. <u>T.B.D.</u>		
	DESIGNED BY <u>SJK</u>		

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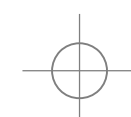
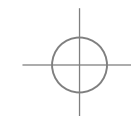
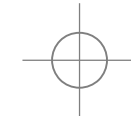
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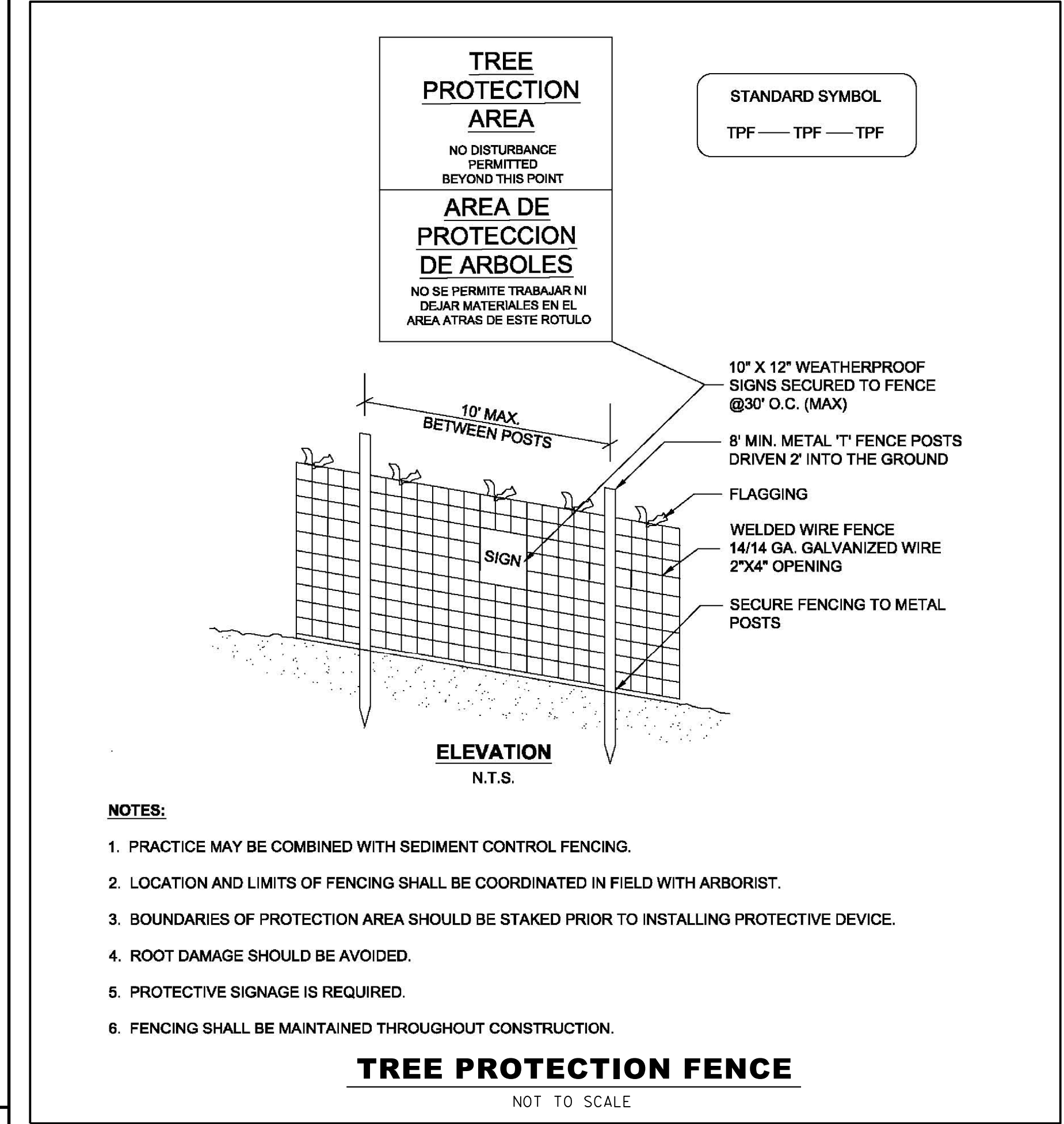
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Sequence of Events for Properties Required to Comply With Forest Conservation Plans, Exemptions from Submitting Forest Conservation Plans, and Tree Save Plans

The property owner is responsible for ensuring all tree protection measures are performed in accordance with the approved final forest conservation plan or tree save plan, and as modified in the field by a Planning Department Forest Conservation Inspector. The measures must meet or exceed the most recent standards published by the American National Standards Institute (ANSI A300).

Pre-Construction

- An on-site pre-construction meeting is required after the limits of disturbance have been staked and flagged and before any land disturbance.
- The property owner must arrange for the meeting and following people must participate at the pre-construction meeting: the property owner or their representative, construction superintendent, International Society of Arboriculture (ISA) certified arborist/Maryland Licensed Tree Expert (representing owner) that will implement the tree protection measures, The Planning Department Forest Conservation Inspector, The Montgomery County Department of Permitting Services (DPS) Sediment Control Inspector, and The City of Takoma Park Arborist. The purpose of this meeting is to verify the limits of disturbance and discuss specific tree protection and tree care measures shown on the approved plan. No land disturbance shall begin before tree protection and stress-reduction measures have been implemented and approved by the Planning Department's Forest Conservation Inspector.
 - Typical tree protection devices include:
 - Chain link fence (four feet high)
 - Super silt fence with wire strung between the support poles (minimum 4 feet high) with high visibility flagging.
 - 14 gauge, 2 inch x 4 inch welded wire fencing supported by steel T-bar posts (minimum 4 feet high) with high visibility flagging.
 - Typical stress reduction measures may include, but are not limited to:
 - Root pruning with a root cutter or vibratory plow designed for that purpose. Trenchers are not allowed, unless approved by the Forest Conservation Inspector
 - Crown Reduction or pruning
 - Watering
 - Fertilizing
 - Vertical mulching
 - Root aeration systems

Measures not specified on the Tree Save Plan may be required as determined by the Forest Conservation Inspector in coordination with the property owner's arborist.

- A Maryland Licensed Tree expert must perform, or directly supervise, the implementation of all stress reduction measures. Documentation of the process (including photographs) may be required by the Forest Conservation Inspector, and will be determined at the pre-construction meeting.
- Temporary tree protection devices must be installed per the approved Forest Conservation Plan, Exemption Plan, or Tree Save Plan and prior to any land disturbance. The Forest Conservation Inspector, in coordination with the DPS Sediment Control Inspector, may make field adjustments to increase the survivability of trees and forest shown as saved on the approved plan.

- Tree protection fencing must be installed and maintained by the property owner for the duration of construction project and must not be altered without prior approval from the Forest Conservation Inspector. All construction activity within protected tree and forest areas is prohibited. This includes the following activities:
 - Parking or driving of equipment, machinery or vehicles of any type.
 - Storage of any construction materials, equipment, stockpiling, fill, debris, etc.
 - Dumping of any chemicals (i.e., paint thinner), mortar or concrete remainder, trash, garbage, or debris of any kind.
 - Felling of trees into a protected area.
 - Trenching or grading for utilities, irrigation, drainage, etc.

- Forest and tree protection signs must be installed as required by the Forest Conservation Inspector. The signs must be waterproof and wording provided in both English and Spanish. During Construction

- Periodic inspections will be made by the Forest Conservation Inspector. Corrections and repairs to tree protection devices must be completed within the timeframe given by the Inspector.

- The property owner must immediately notify the Forest Conservation Inspector of any damage to trees, forests, understory, ground cover, and any other undisturbed areas shown on the approved plan. Remedial actions, and the relative timeframes to restore these areas, will be determined by the Forest Conservation Inspector.

Post-Construction

- After construction is completed, but before tree protection devices have been removed, the property owner must request a final inspection with the Forest Conservation Inspector. At the final inspection, the Forest Conservation Inspector may require additional corrective measures, which may include:
 - Removal, and possible replacement, of dead, dying, or hazardous trees
 - Pruning of dead or declining limbs
 - Soil aeration
 - Fertilization
 - Watering
 - Wound repair
 - Clean up of retention areas, including trash removal
- After the final inspection and completion of all corrective measures the Forest Conservation Inspector will request all temporary tree and forest protection devices be removed from the site. Removal of tree protection devices that also operate for erosion and sediment control must be coordinated with both DPS and the Forest Conservation Inspector and cannot be removed without permission of the Forest Conservation Inspector. No additional grading, sodding, or burial may take place after the tree protection fencing is removed.
- Long-term protection measures, including permanent signage, must be installed per the approved plan. Installation will occur at the appropriate time during the construction project. Refer to the approved plan drawing for the long-term protection measures to be installed.

TS-06

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07/21/2021

DATE

NO.	REVISION	DATE	BY

60% PLANS

NOVEMBER 2021

CITY OF TAKOMA PARK

NEW AVE BIKEWAY, SECTION B

MD 650 (NEW HAMPSHIRE AVENUE)

POPLAR AVE TO AUBURN AVE

SHA TRACKING NO. 20-AP-MO-020-XX

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DRAWING NO. TS-06 6 OF 6 SHEET NO. 44 OF 44

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